



PLANNING & ZONING BOARD AGENDA

Wednesday, January 14, 2026

Broadcast on Cable TV Channel 16
and northmetrotv.com/lino-lakes-stream

ADVISORY BOARD MEETING, 6:30 P.M. Council Chambers (televised)

1. Call to Order and Roll Call
2. Pledge of Allegiance
3. Public Comment (*sign-in prior to start of meeting per Rules of Decorum*)
4. Setting the Agenda: Addition or Deletion of Agenda Items
5. Approval of Minutes: December 10, 2025
6. Action Items
 - A. 2245 Tele Drive 3-Season Porch Setback Variance
 - B. Pine Oaks PUD Concept Plan Review
7. Discussion Items
 - A. Metropolitan Council 2050 System Statement
 - B. Public Works Design Update
 - C. Project Updates

ADJOURNMENT

**CITY OF LINO LAKES
PLANNING & ZONING BOARD MINUTES**

DATE:	December 10, 2025
TIME STARTED:	6:30 P.M.
TIME ENDED:	8:34 P.M.
MEMBERS PRESENT:	Isaac Wipperfurth, Nathan Vojtech, Michael Root, Suzy Guthmueller, Perry Laden, Patrick Kohler
MEMBERS ABSENT:	Neil Evenson
STAFF PRESENT:	Michael Grochala, Katie Larsen, Diane Hankee, Jessica Eller, Marissa Ertel

1. CALL TO ORDER AND ROLL CALL

Chair Root called the Lino Lakes Planning & Zoning Board meeting to order at 6:30 p.m. on December 10, 2025.

2. PLEDGE OF ALLEGIANCE

3. PUBLIC COMMENT

Chair Root declared public comment open at 6:31 p.m.

There was no one present for public comment.

Mr. Vojtech motioned to close public comment at 6:31 p.m. Motion was seconded by Mr. Laden. Motion carried 6 – 0.

4. APPROVAL OF AGENDA

The agenda was approved as presented.

5. APPROVAL OF MINUTES

Mr. Vojtech moved to approve the November 12, 2025 meeting minutes. Motion seconded by Mr. Kohler. Motion carried 6 – 0.

6. ACTION ITEMS

DRAFT MINUTES

A. Continued Public Hearing: Peltier Ponds Comprehensive Plan Amendment, Rezoning, PUD Preliminary Plan and Preliminary Plat

The applicant, U.S. Home, LLC (aka Lennar), is proposing a master planned residential development located west of CSAH 54 (20th Ave N) across from Watermark, east of Peltier Lake, and north of the City of Centerville border. The development contains four (4) parcels totaling approximately 125 gross acres and 282 housing units. The proposed development is called Peltier Ponds.

The Planning & Zoning Board held a public hearing on July 9, 2025 and recommended continuing the public hearing to allow the applicant time to provide additional information. The Board continued the public hearing again on August 13, 2025, September 10, 2025, October 8, 2025, and November 12, 2025. The applicant submitted revised PUD Preliminary Plans/Preliminary Plat on November 3, 2025.

Josh Metzger, U.S. Home, LLC, presented and was available to answer questions.

Mr. Wipperfurth had questions regarding Peltier Lake Drive and whether this needs to be shown on the plans. Ms. Larsen stated that this is shown in the civil plans.

Mr. Wipperfurth wanted to know the plan for the trail along Peltier Lake. Ms. Larsen stated this would be a City project using the funds from the park dedication fees. The construction of the trail would be completed along with phase four.

Mr. Wipperfurth asked who would pay for the added roundabout. Mr. Grochala stated that this would be a decision between the developer, the City and the County. Mr. Wipperfurth wanted to confirm that the street width would not be changed.

Mr. Wipperfurth had questions about the extension of a trail along 20th. Ms. Hankee stated that this would potentially be completed with the development or along with County Road reconstruction projects.

Mr. Kohler had concerns about stormwater management and not meeting standards. Ms. Hankee did confirm that the developer has met standards since the first meeting.

Mr. Kohler had concerns about water supply for the area. Ms. Hankee stated that mentioning the water supply is for the developer. Ms. Hankee stated that the City and engineering is actively working on getting Well No. 7 running. Mr. Grochala stated that the City is watching other wells being drilled closely in the surrounding area.

Mr. Kohler wanted to speak on the neighborhood meeting concerns regarding water and drainage issues. Ms. Hankee stated that along Rehbein Street there are issues the City is aware of. The developer will be responsible for run-off from the site.

Mr. Kohler had concerns regarding the engineering comments from WSB. Ms. Hankee stated that several of the comments have been addressed by the developer, and the remaining are not of high concern.

Mr. Vojtech wanted to speak about the guest parking for the townhomes and worries that much of the guest parking is located to the South. Ms. Larsen stated that the developer is meeting the parking requirements.

Mr. Vojtech spoke about the shoreline and future park and pavilions.

Chair Root had questions about the comprehensive plan amendment and the lower limit changing from 383 to 382. Ms. Larsen explained that this is a guideline and the City worked along with the developer to meet the requirements.

Chair Root had questions about the roundabout. Mr. Root wanted to confirm that the developer, regardless of the roundabout, would have to address traffic. Ms. Larsen confirmed that the developer would address traffic and the City and County will be working on the roundabout.

Chair Root wanted to address tree preservation, specifically along the North lot line. Ms. Larsen stated that not all trees will be replaced, but tree preservation is a priority and that the City will encourage trees be added along the north lot line.

Mr. Kohler had concerns about the homes that would be along the powerline easements. Ms. Larsen stated that the lots do meet requirements and have mentioned this to the developer.

Ms. Guthmueller had questions about the lots on the floodplains and worries for the two lots near the lake. Ms. Larsen stated that the lots are not considered too close to the lake.

Mr. Wipperfurth had concerns about the existing homes on the site and the driveway access. Ms. Larsen stated that the developer will need to work with the homeowners, and that one of those homeowners is the owner of the land.

Mr. Metzger spoke on the trees along the north lot line and stated that they would be removing trees that are already dead or dying. This is also to ensure that the lots can be properly graded so that there is no runoff to the existing properties.

Mr. Metzger stated they are already in contact with the existing homeowners and are working on the driveway concerns.

Chair Root wanted to address concerns from the neighborhood meeting and how they plan to address those concerns. Mr. Metzger stated that they have not changed from the neighborhood meeting, rather the existing concerns regarding drainage and brush. Mr. Metzger stated the runoff would be reduced.

Chair Root wanted to comment on the entry sign and the renderings and wanted to suggest adding "Lino Lakes" to the sign.

Chair Root opened the public hearing at 7:45 p.m.

Jerry L'Allier, 7601 Gordon Avenue, had concerns about the trees along the north lot line and not being able to view the lake. Mr. L'Allier stated that the current trees are doing a good job of preventing flooding and would like to see more screening. Mr. L'Allier does not want to have to pay for his own trees and believes the developer should have to pay. Mr. L'Allier also had concerns about traffic in the area and the removal of the road along the lake. Mr. L'Allier believes this would make the area more dangerous. Mr. L'Allier does not understand why the water supply is not being addressed and believes we should not approve phases three and four until the well is in place.

Mr. Laden motioned to close the public hearing at 7:48 p.m. Motion was seconded by Ms. Guthmueller. Motion carried 6 – 0.

Mr. Metzger addressed the concerns regarding the trees, he stated there will be 70 trees planted along there and many of the existing trees will not be removed.

Chair Root wanted to address the traffic concerns regarding closing Peltier Lake Drive. Ms. Hanke clarified that the street is not being shut down, rather rerouted through the site.

Chair Root addressed the water supply and how the City could approve development if there would not be guaranteed supply for each phase. Ms. Larsen wanted to clarify that this is a preliminary plat, and that before each phase this will be presented before the board for each final plat where water supply would be addressed at that time.

Mr. Laden wanted to speak on the architectural standards. Mr. Laden is fine with the flexibility for the larger lot sizes. Mr. Laden was not in favor of flexibility regarding the garage façade and size. Mr. Laden would not be in favor of this for a single floor plan. Mr. Laden wanted to make sure that the garage window on the rambler is included in the recommendations.

Chair Root wanted to clarify the recommendations. Mr. Laden stated that he would be fine with the flexibility for the 55-foot lots if all flexibility is not on one single layout and would not want the houses of the same layout to be next to the other.

Mr. Laden also spoke on the garage door styles and would like to see more variety.

Mr. Metzger stated that since the development of Watermark there is more variety in garage door styles.

Mr. Kohler motioned to approve the Peltier Ponds Comprehensive Plan Amendment, Rezoning, PUD Preliminary Plan and Preliminary Plat with staff recommendations. Motion seconded by Mr. Wipperfurth. Motion failed 3 – 3.

Mr. Vojtech motioned to approve the Peltier Ponds Comprehensive Plan Amendment, Rezoning, PUD Preliminary Plan and Preliminary Plat with staff recommendations and the following Board recommendations:

- Garage size must be a minimum of 520 feet for the 55-foot lots
- Garages cannot be forward for the 55-foot lots

Motion seconded by Mr. Laden. Motion failed 2 – 4.

Mr. Laden motioned to approve the Peltier Ponds Comprehensive Plan Amendment, Rezoning, PUD Preliminary Plan and Preliminary Plat with staff recommendations and the following Board recommendations:

- Only two of the four PUD flexibility be allowed per 55-foot lot
- Windows over the garage for single-story homes
- Adjacent houses on the 55-foot lots cannot have protruding garages
- Garage door windows should have variety

Motion seconded by Mr. Root. Motion failed 1 – 5.

After much discussion regarding architectural requirements for the 55-foot lots, the Board could not come to a consensus about a recommendation.

B. Public Hearing: Lyngblomsten PUD Amendment #3-Skilled Nursing Care Center

The applicant, Lyngblomsten at Lino Lakes, LLC, submitted a Land Use Application for PUD Amendment #3. They are requesting an amendment to the PUD Preliminary Plan (“PUD Amendment”) to change the 2-story skilled nursing care center to a 3-story skilled nursing care center. The property address is 6070 Blanchard Boulevard.

Mr. Wipperfurth wanted to confirm the roof style. Ms. Larsen stated that the roof style does not change with this PUD amendment.

Judd Fenlon, Grand Real Estate Advisors, was available for questions.

Mr. Laden wanted to confirm that this skilled nursing unit is not already being done with the existing units. Mr. Fenlon confirmed that this is not being done in the existing buildings.

Chair Root opened the public hearing at 8:31 p.m.

There was no one present to speak.

Mr. Vojtech motioned to close the public hearing. Motion was seconded by Mr. Kohler. The public hearing was closed at 8:31 p.m.

Mr. Wipperfurth moved to approve the Lyngblomsten PUD Amendment #3 – Skilled Nursing Care Center with the staff and board recommendations. Motion seconded by Mr. Vojtech. Motion carried 6 – 0.

7. DISCUSSION ITEMS

A. Project Updates

Ms. Larsen provided project updates to the Board.

Mr. Vojtech moved to adjourn the meeting. Motion seconded by Mr. Laden. Meeting adjourned at 8:34 p.m.

Respectfully submitted,
Marissa Ertel – Community Development Administrative Assistant

**PLANNING & ZONING BOARD
AGENDA ITEM 6A**

STAFF ORIGINATOR: Katie Larsen, City Planner

MEETING DATE: January 14, 2026

TOPIC: 2245 Tele Drive 3-Season Porch Setback Variance

CASE NUMBER: VAR2025-002

APPLICANT: Lake to Lake Construction
Attn: Todd Wanka
2152 3rd Street, Suite A
White Bear Lake, MN 55110

OWNER: Kurt and Caryn Henriksen
2245 Tele Drive
Lino Lakes, MN 55038

INTRODUCTION

The applicant, Todd Wanka, is requesting a setback variance for the purpose of constructing a 3-season porch. The property address is 2245 Tele Drive.

Tentative Review Schedule:

Complete Application Date:	December 4, 2025
60-Day Deadline:	February 2, 2026
Environmental Board Meeting:	N/A
Park Board Meeting:	N/A
Planning & Zoning Board Meeting:	January 14, 2026
City Council Work Session:	February 2, 2026
City Council Meeting:	February 9, 2026

BACKGROUND

The Land Use Application is for the following:

- Variance

This staff report is based on the following submittals:

- Applicant Narrative
- Survey prepared by Lot Surveys Company, Inc. dated June 24, 1997
- Construction Plans

ANALYSIS

The applicant is proposing to construct a 16' x 16' 3-season porch off the back of the existing house. The lot is a corner lot. The rear lot line is opposite the front lot line along Tele Drive. The rear lot line is not parallel to the front lot line.

Zoning and Setback Requirements

The property is zoned R-1, Single Family Residential. The required rear lot line principal building setback is 30ft. The 3-season porch is considered part of the principal building. Per the survey submitted with the variance application, the northeast corner of the porch will encroach 3.5ft into the required 30ft setback. The northwest corner of the porch will not encroach into the setback. No portion of the porch encroaches into the drainage & utility easement.

The applicant notes in the narrative that the "resident's desired design cannot shift east or west due to the existing windows and doors currently on the house".

Option

A smaller 3-season porch could be constructed that meets setback requirements.

Criteria and Findings of Fact

Per City Code Section 1007.018(3)(a) *Criteria and findings of fact*. No variance shall be granted unless it meets all the criteria in divisions 1. through 6. below, or unless division 7. below applies. The city shall make findings regarding compliance with these criteria.

1. The variance shall be in harmony with the general purposes and intent of this chapter.

City Code Section 1007.000(2) details the purpose and intent of Chapter 1007: Zoning Code. The variance is in harmony with the general purposes and intent of this chapter.

2. The variance shall be consistent with the official City Comprehensive Plan.

Per the Comprehensive Plan, the property is guided for Low Density Residential land use.

The variance request is consistent with residential land use. The property is intended for single family detached dwellings with 3-season porches.

3. There shall be practical difficulties in complying with this chapter. "Practical difficulties," as used in connection with the granting of a variance, means that the property owner proposes to use the property in a reasonable manner not permitted by this chapter. Economic considerations alone do not constitute practical difficulties. Practical difficulties include, but are not limited to, inadequate access to direct sunlight for solar energy systems.

The property owner proposes to use the property in a reasonable manner not permitted by this chapter. The practical difficulty is created by the angle of the rear lot line. Only a portion of the 3-season porch encroaches into the setback.

4. The plight of the landowner shall be due to circumstances unique to the property not created by the landowner.

The plight of the landowner is due to the circumstances unique to the property due to the angle of the rear lot line. The plight was not created by the landowner.

5. The variance shall not alter the essential character of the locality.

The variance will not alter the essential character of the of the locality. The residential neighborhood consists of single family detached dwellings with additions.

6. A variance shall not be granted for any use that is not allowed under the ordinance for property in the zoning district where the subject site is located.

The variance is not being granted for a use that is not allowed under the ordinance. Single family detached dwellings with 3-season porches are permitted uses in the R-1, Single Family Residential zoning district.

7. In accordance with M.S. § 462.357, Subd. 6, variances shall be granted for earth sheltered construction as defined in M.S. § 216C.06, Subd. 14, when in harmony with the zoning ordinance.

Not applicable.

RECOMMENDATION

Staff recommends approval of the following setback variance for the 2245 Tele Drive 3-season porch:

- A 3.5 foot variance to reduce the rear lot line principal building setback from 30 feet to 26.5 feet as shown on the submitted survey.

ATTACHMENTS

1. Site Location & Aerial Map
2. Applicant Narrative
3. Survey prepared by Lot Surveys Company, Inc. dated June 24, 1997
4. Construction plans



1 in = 60 Ft

Address Labels

 Parcels





Client: Kurt and Caryn Henriksen
Property: 2245 Tele dr
Lino Lakes , MN 55038

Operator: WANKALTL

Estimator: Todd Wanka
Company: Lake to Lake Construction
Business: 2152 suite A 3rd
White Bear Lake, MN 55110

Type of Estimate:

Date Entered: 7/17/2025

Date Assigned:

Price List: MNMN8X_JUL25

Labor Efficiency: Restoration/Service/Remodel

Estimate: HENRIKSEN

Scope:

- 16 x 16 3 season Porch with fireplace.
- The proposed new structure Exceeds set back (30') from the North lot line. The new proposed structures North East corner falls at 26' 6" from set back, The North west corner however fall in line at 32' setback. The set back comes through at a diagonal. The Residence desired design cannot shift East or west due to Existing windows and Doors correctly on the House.
- tree Cover between the Existing North side neighbor will cover a majority of the new site lines of the new structure
- Plans Have been sent Electronically also to.

Klarsen@Linolakes.US.

Established in 1982
LOT SURVEYS COMPANY, INC.
 LAND SURVEYORS

REGISTERED UNDER THE LAWS OF STATE OF MINNESOTA
 7601 73rd Avenue North
 Minneapolis, Minnesota 55428
 618-600-3083
 Fax No. 600-3688

Surveyors Certificate

T & D HOMES

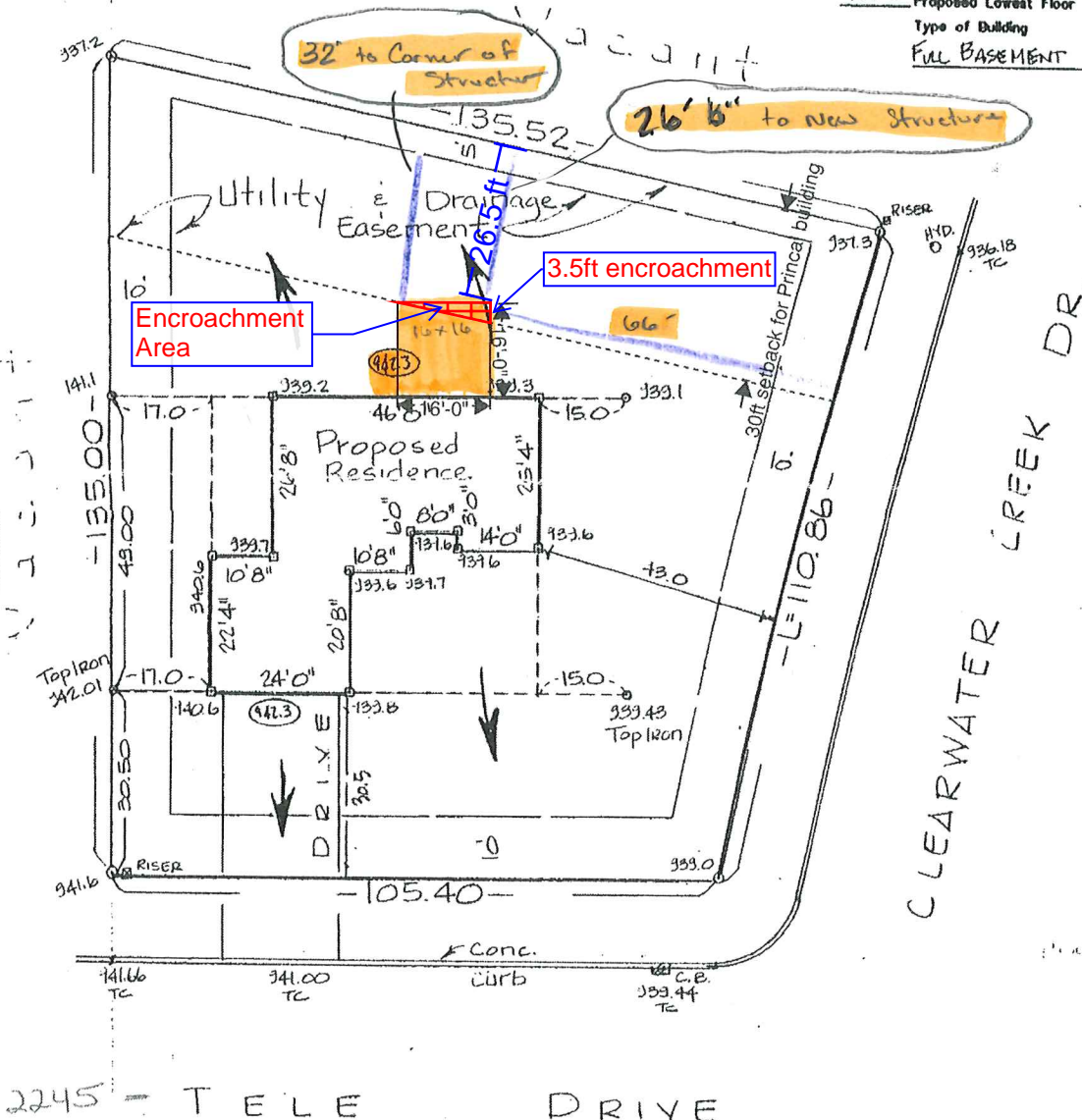
Property located in Section
 25, Township 31, Range 22,
 Anoka County, Minnesota

INVOICE NO. 47029
 F.B.NO. 769-77
 SCALE: 1" = 20'

- Denotes Iron Monument
- Denotes Wood Hub Set for excavation only
- +000.0 Denotes Existing Elevation
- ⊙000.0 Denotes Proposed Elevation
- Denotes Surface Drainage

NOTE: Proposed grades are subject to results of soil tests.
 Proposed building information must be checked with approved building plan and development or grading plan before excavation and construction.

943.0 Proposed Top of Block
 942.5 Proposed Garage Floor
 935.0 Proposed Lowest Floor
 Type of Building
 FULL BASEMENT



2245 - TELE DRIVE

Lot 7, Block 2, CLEARWATER CREEK 2ND ADDITION

The only easements shown are from plots of record or information provided by client.
 We hereby certify that this is a true and correct representation of a survey of the boundaries of the above described land and the location of all buildings and visible encroachments, if any, from or on said land.
 Surveyed by us this 24th day of June 19 97

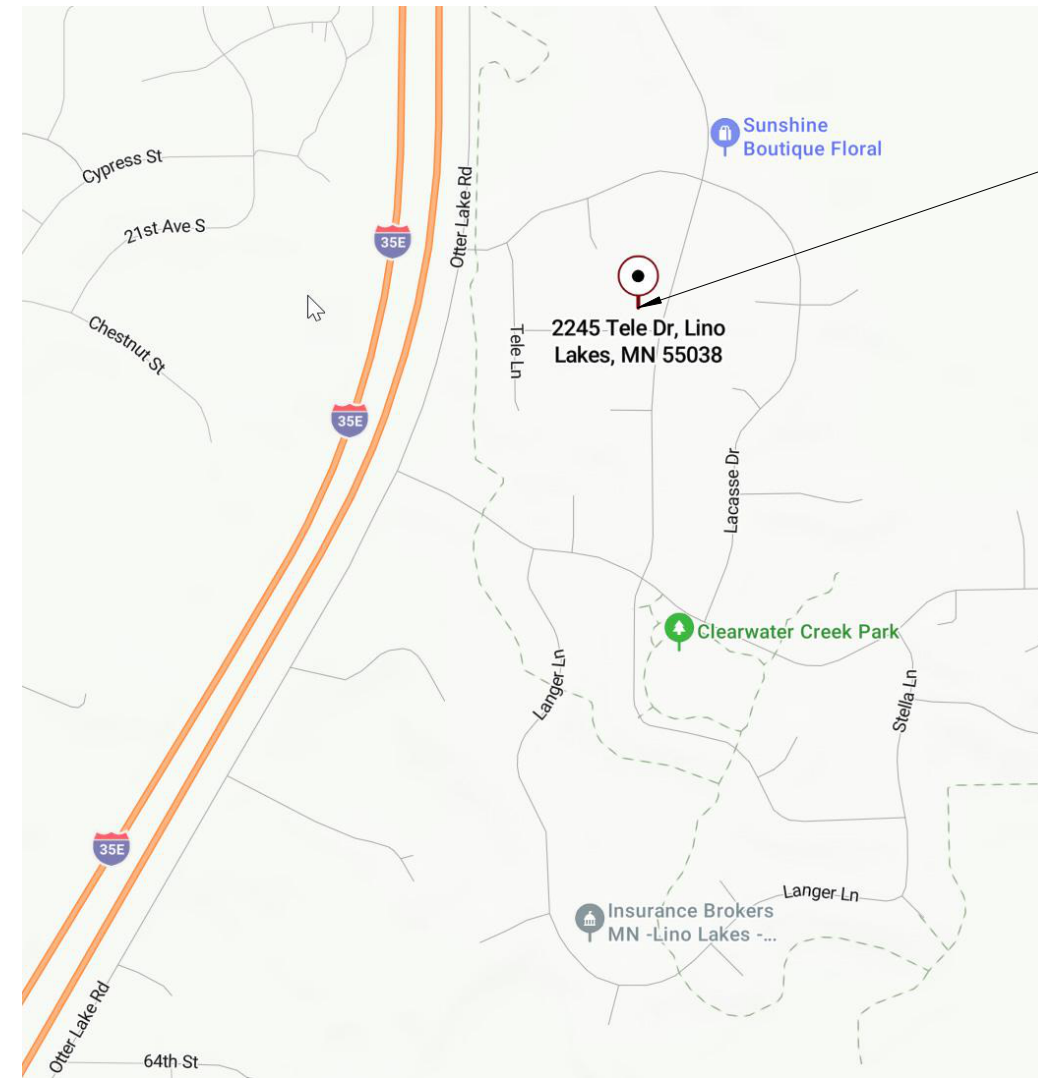
Signed *Charles F. Anderson*
 Charles F. Anderson, Minn. Reg. No. 21753

Hendrickson Residence

2245 Tele Drive
Lino Lakes, MN 55038
PROJECT PERMIT
10.03.2025

SHEET INDEX

00-TITLE	
A0.0	TITLE SHEET
01-ARCHITECTURE	
A1.0	DEMO FLOOR PLAN AND NEW FLOOR PLAN
A2.0	DEMO ROOF PLAN AND NEW ROOF PLAN
A3.0	EXTERIOR ELEVATIONS
A4.0	BUILDING SECTION AND EXTERIOR DETAILS
A5.0	INTERIOR ELEVATIONS
A6.0	FRAMING PLANS

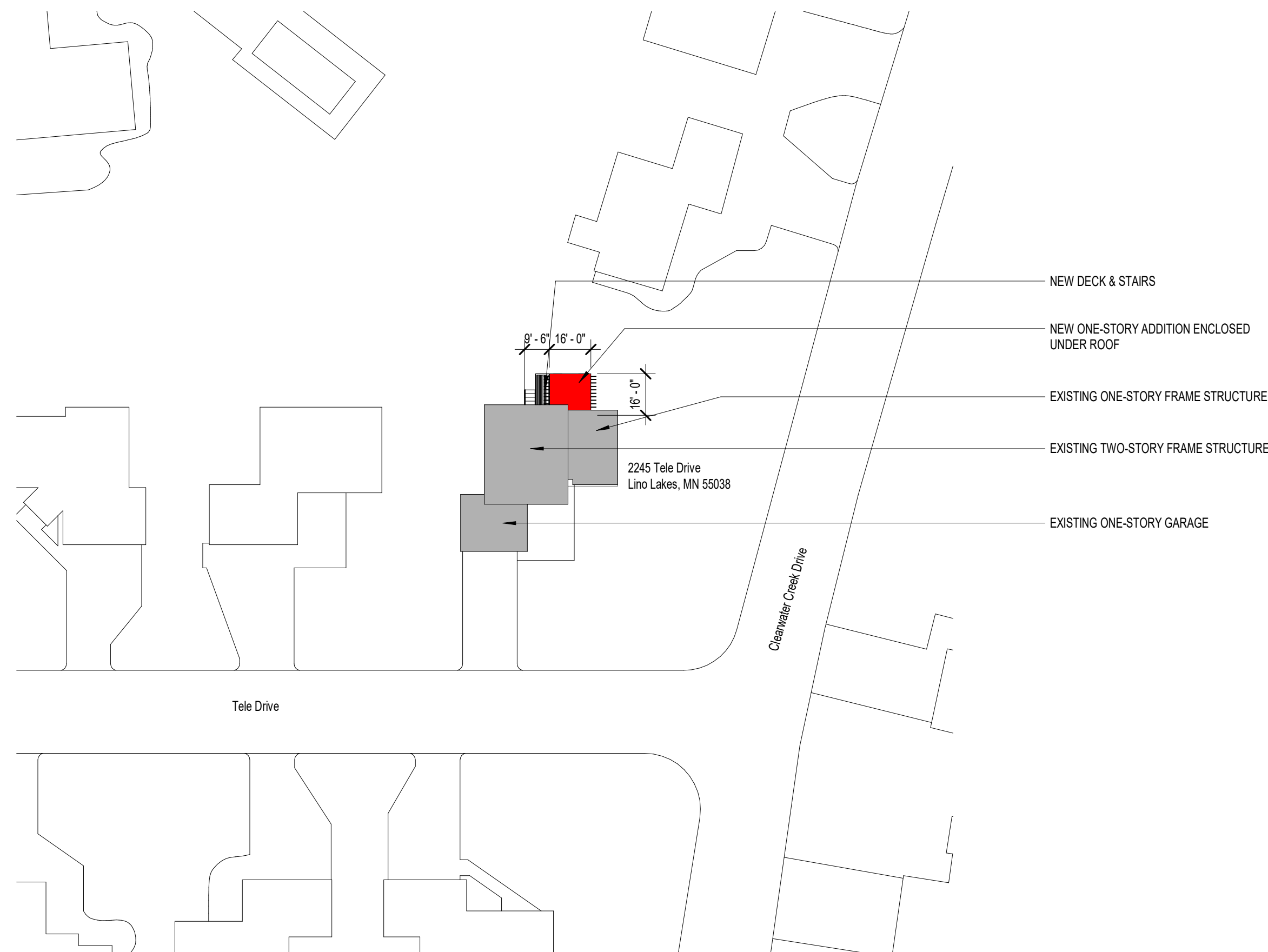
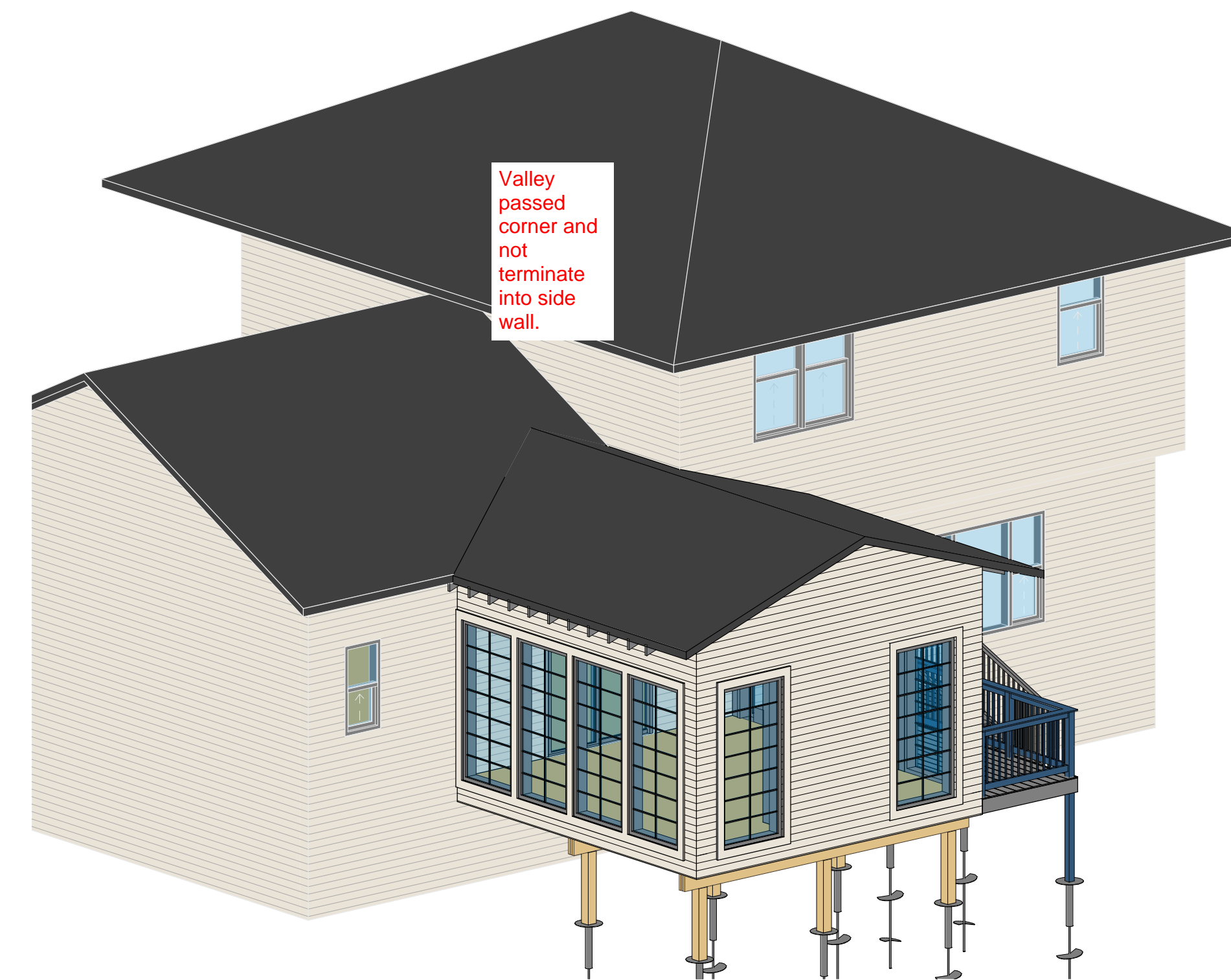
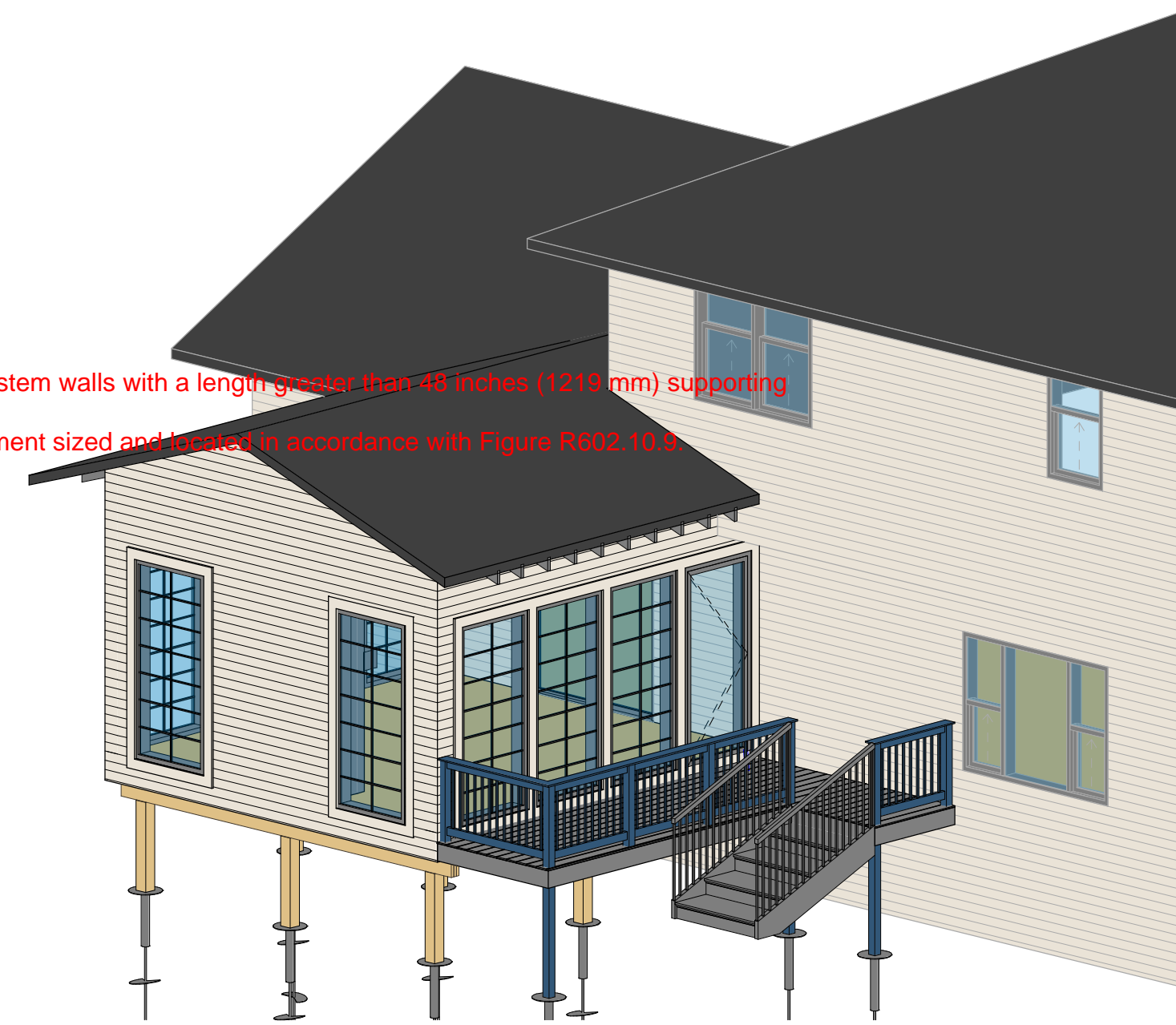


PROJECT SITE

2. Please submit Brace wall panel support info- I have copied the code section below at is applicable to this request.
R602.10.9 Braced wall panel support.

Braced wall panel support shall be provided as follows:

- 1.1. Cantilevered floor joists complying with Section R502.3.3 shall be permitted to support braced wall panels.
- 2.2. Raised floor system post or pier foundations supporting braced wall panels shall be designed in accordance with accepted engineering practice.
- 3.3. Masonry stem walls with a length of 48 inches (1219 mm) or less supporting braced wall panels shall be reinforced in accordance with Figure R602.10.9. Masonry stem walls with a length greater than 48 inches (1219 mm) supporting braced wall panels shall be constructed in accordance with Section R403.1 Methods ABW and PFH shall not be permitted to attach to masonry stem walls.
- 4.4. Concrete stem walls with a length of 48 inches (1219 mm) or less, greater than 12 inches (305 mm) tall and less than 6 inches (152 mm) thick shall have reinforcement sized and located in accordance with Figure R602.10.9.
3. Please submit wall brace information Per R602.10
4. Submit Preliminary report for Helical footings.
5. Provide beam calcs for the 3 ply 9 1/2" LVL
6. Provide header/beam information for bearing walls of porch
7. Provide me header information for end gable wall openings.
8. Clarification on what you are proposing for the roof system? 1/A4.0 states engineered roof trusses but all the other details showing Rafters.
9. What is supporting the stairs?
10. Provide window information
11. Deck support information Beam-joist
12. Valley terminated into side wall
13. can not have foam insulation between framing and subfloor.



Hendrickson Residence

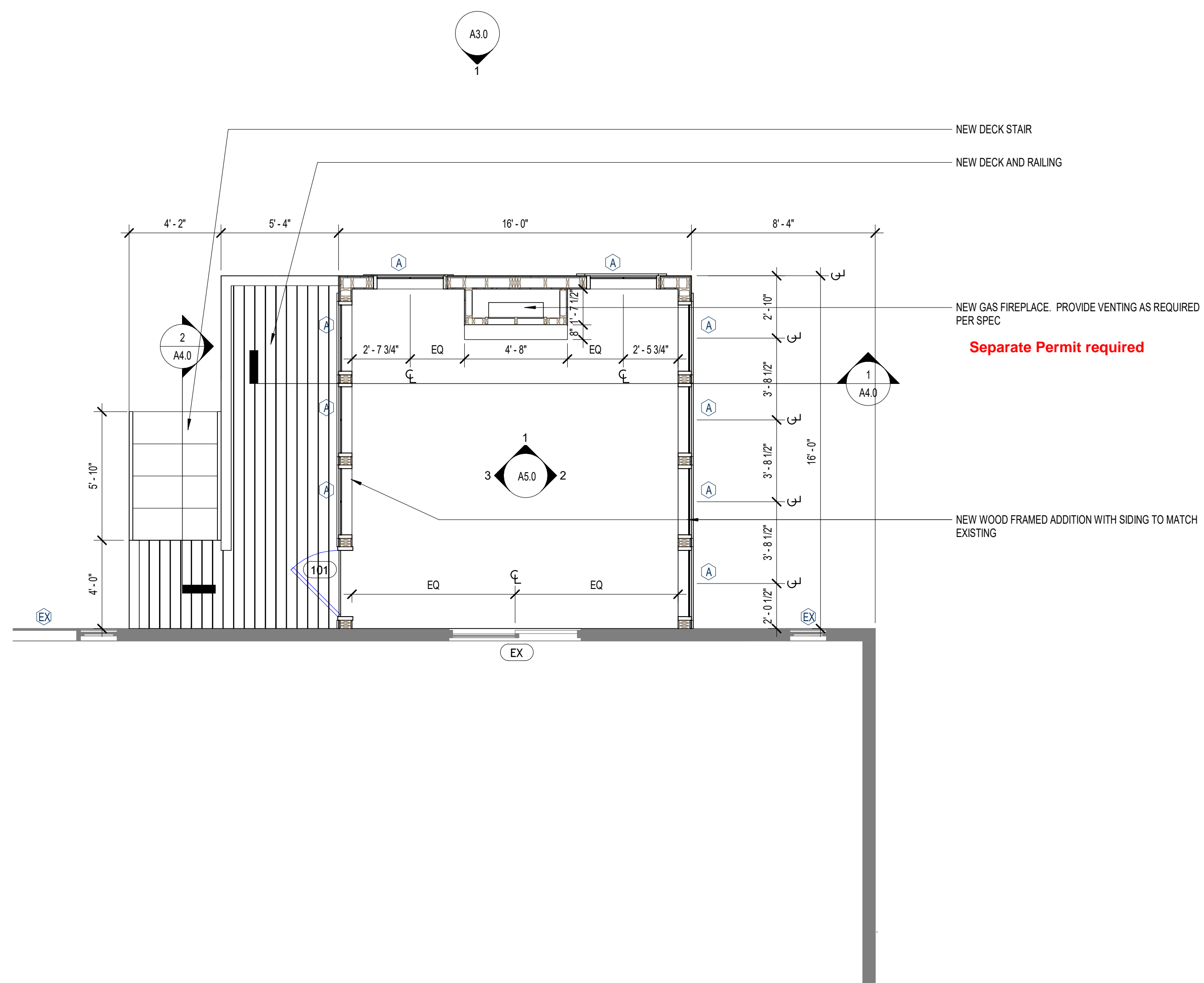
TITLE SHEET

Project Number 1023-02

Date 10.03.2025

A0.0

Scale 1" = 40'-0"



1 FLOOR PLAN - LEVEL 1
SCALE: 1/4" = 1'-0"



2 DEMOLITION FLOOR PLAN - LEVEL 1
SCALE: 1/4" = 1'-0"

Hendrickson Residence

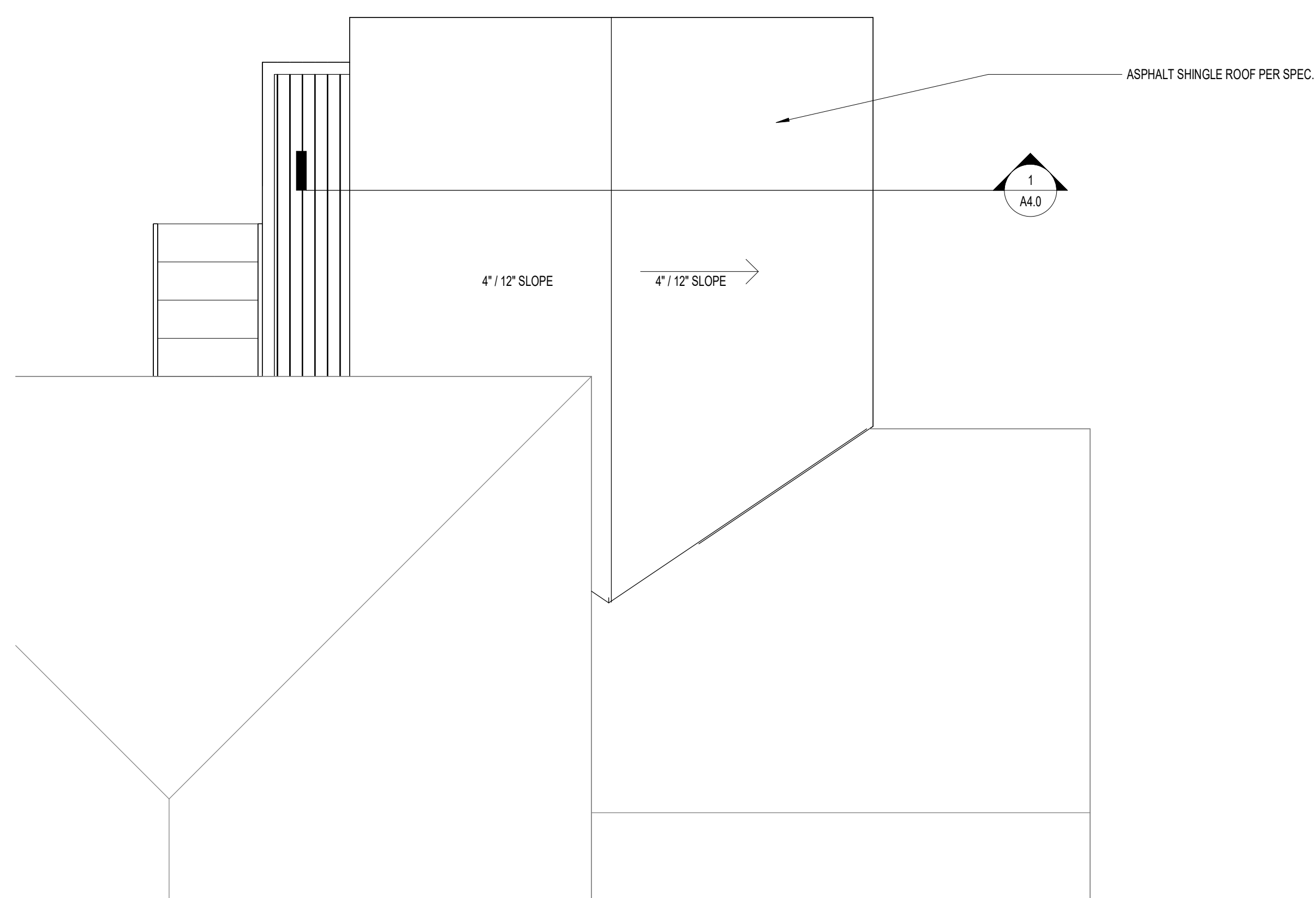
DEMO FLOOR PLAN AND
NEW FLOOR PLAN

Project Number 1023-02

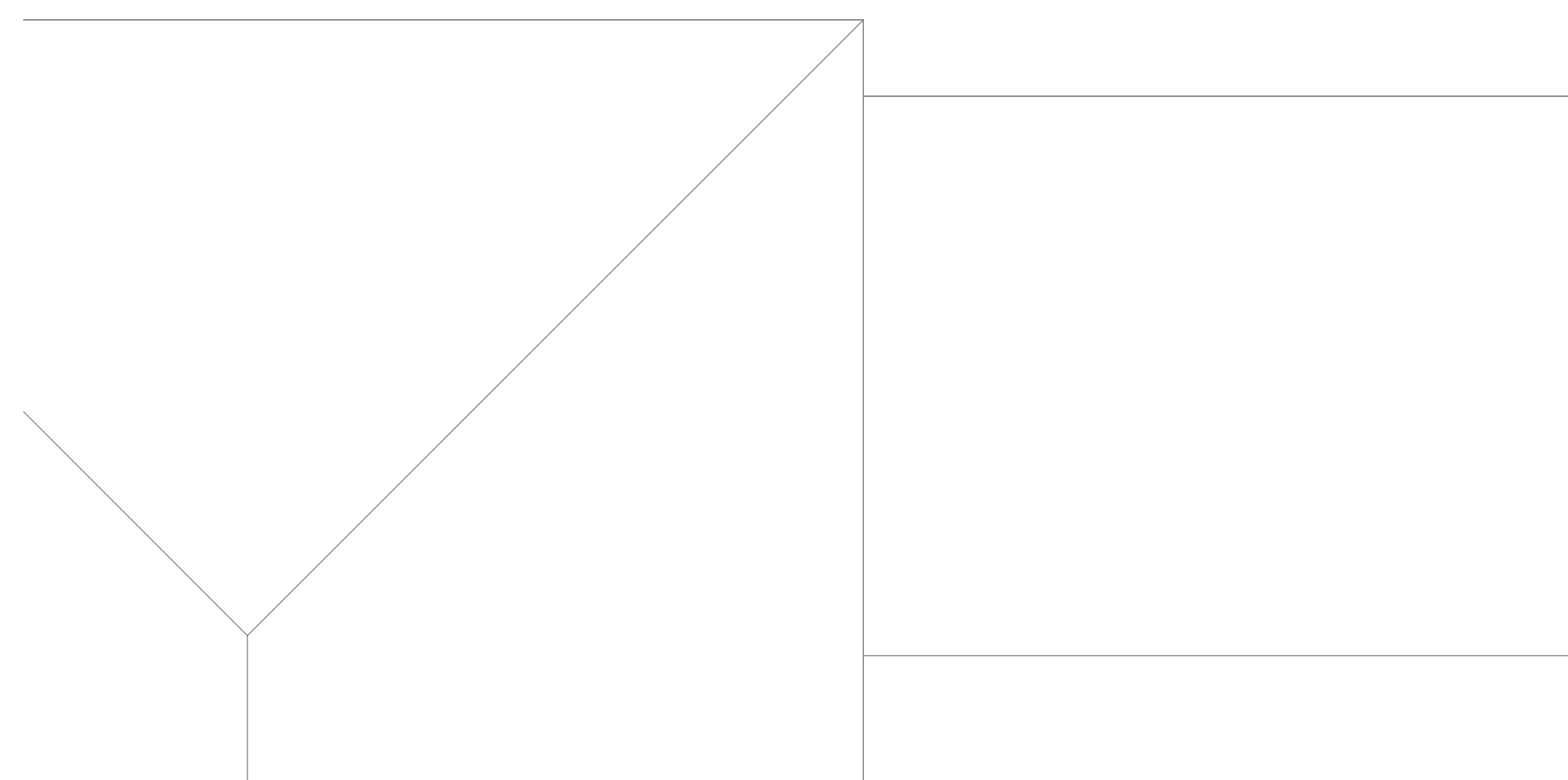
Date 10.03.2025

A1.0

Scale 1/4" = 1'-0"



2 ROOF PLAN
SCALE: 1/4" = 1'-0"



1 DEMOLITION ROOF PLAN
SCALE: 1/4" = 1'-0"

Hendrickson Residence

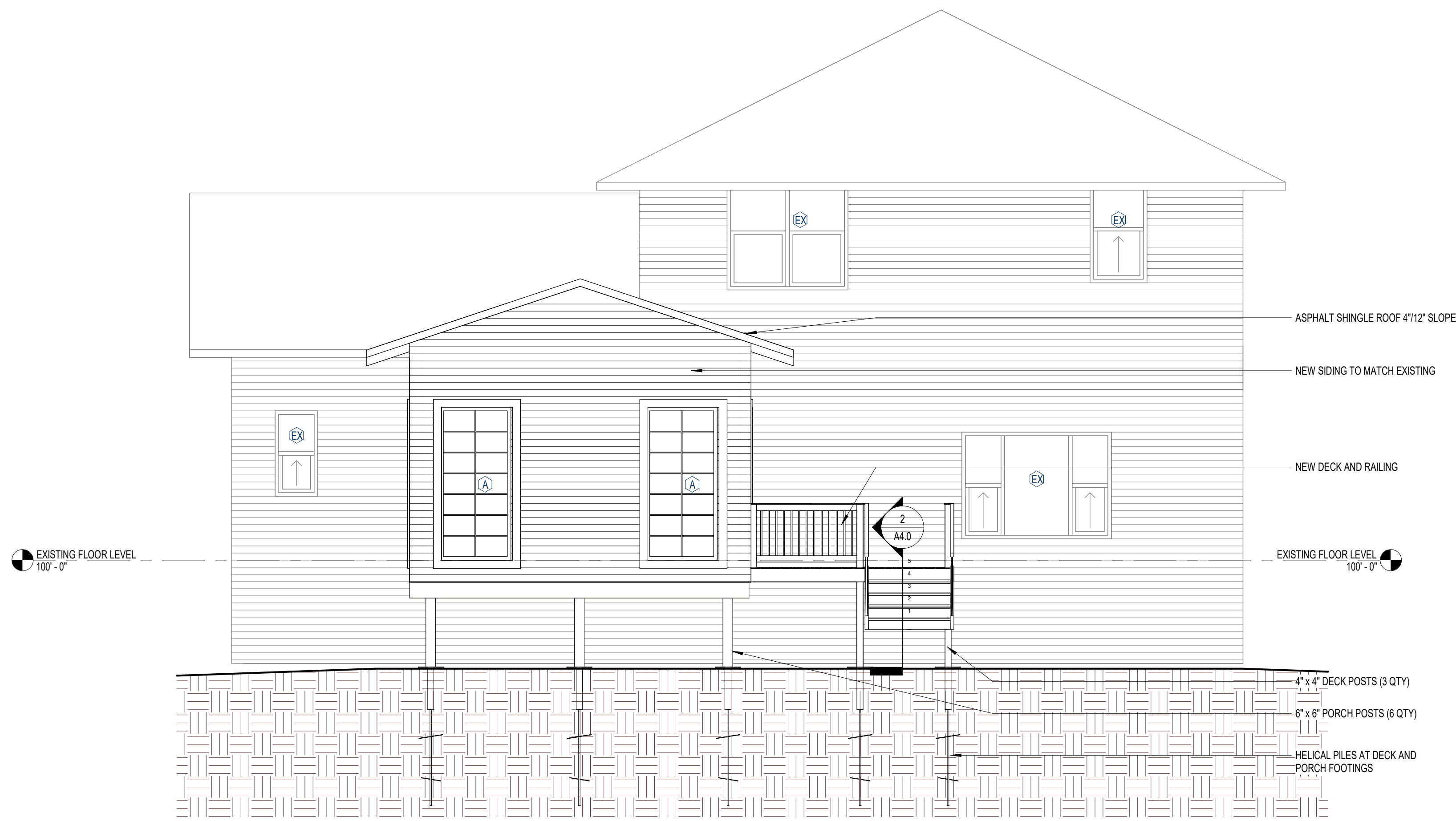
DEMO ROOF PLAN AND
NEW ROOF PLAN

Project Number 1023-02

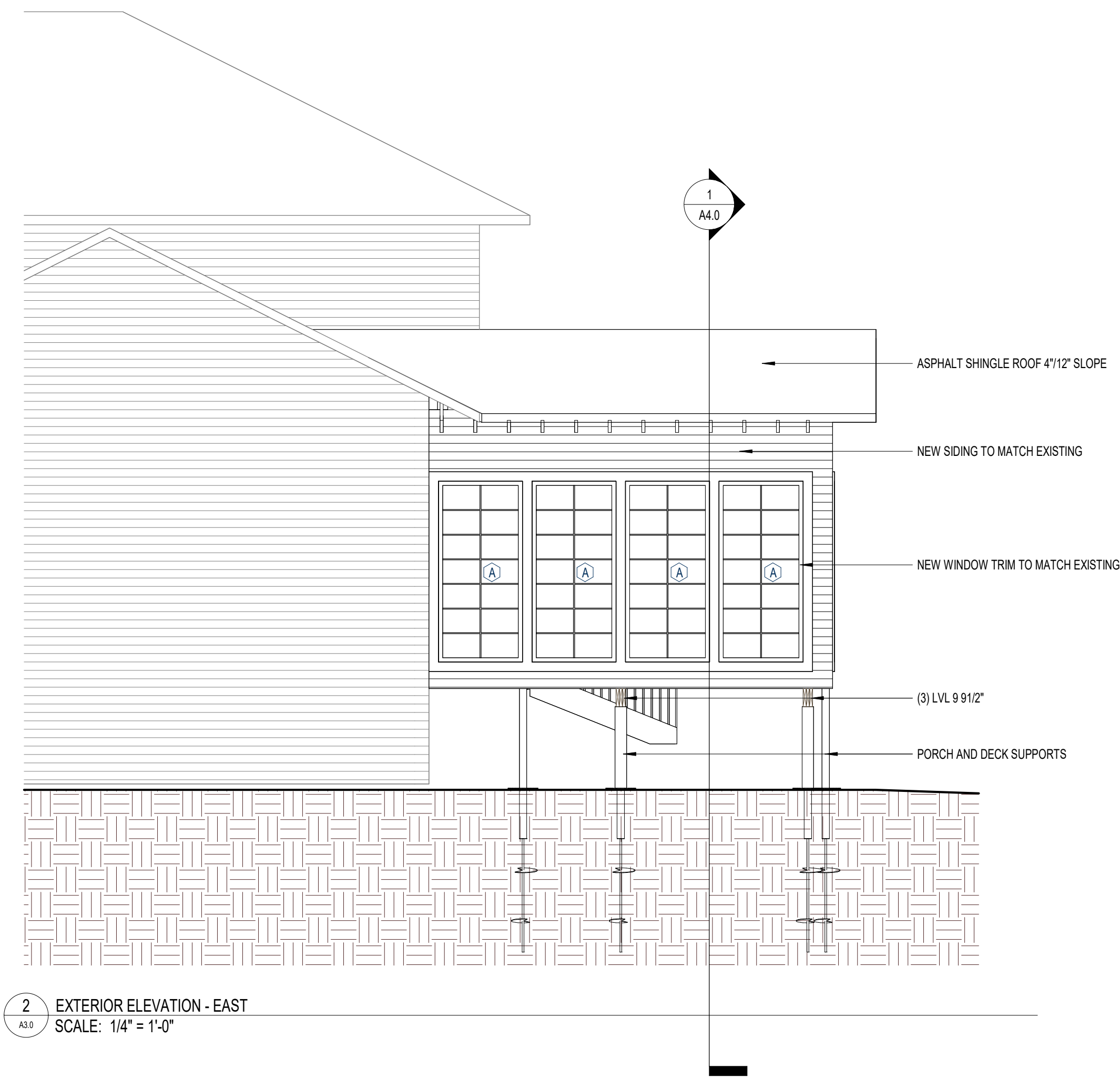
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A2.0

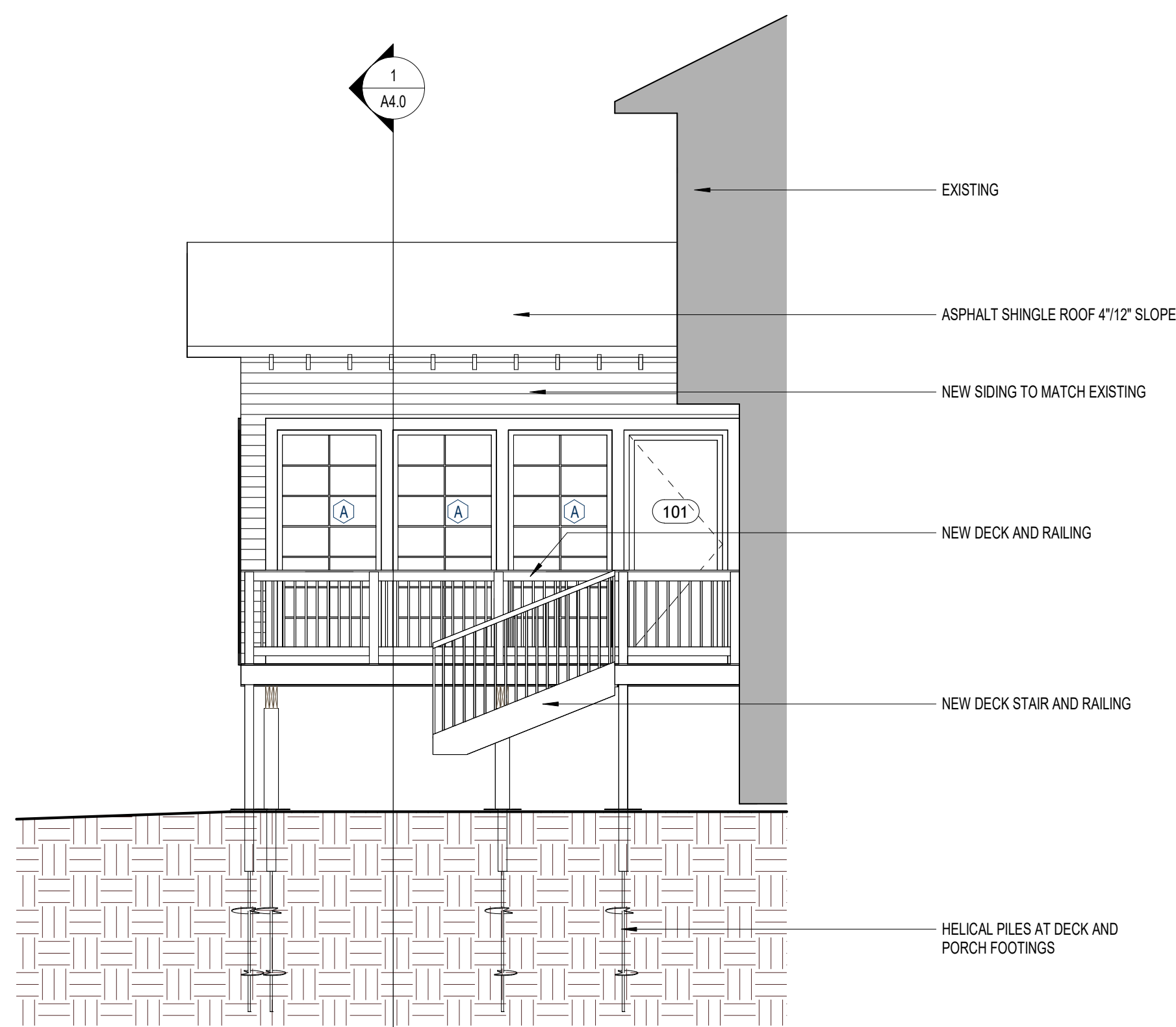
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1
A3.0 EXTERIOR ELEVATION - NORTH
SCALE: 1/4" = 1'-0"



2
A3.0 EXTERIOR ELEVATION - EAST
SCALE: 1/4" = 1'-0"



3
A3.0 EXTERIOR ELEVATION @ WEST
SCALE: 1/4" = 1'-0"

Hendrickson Residence

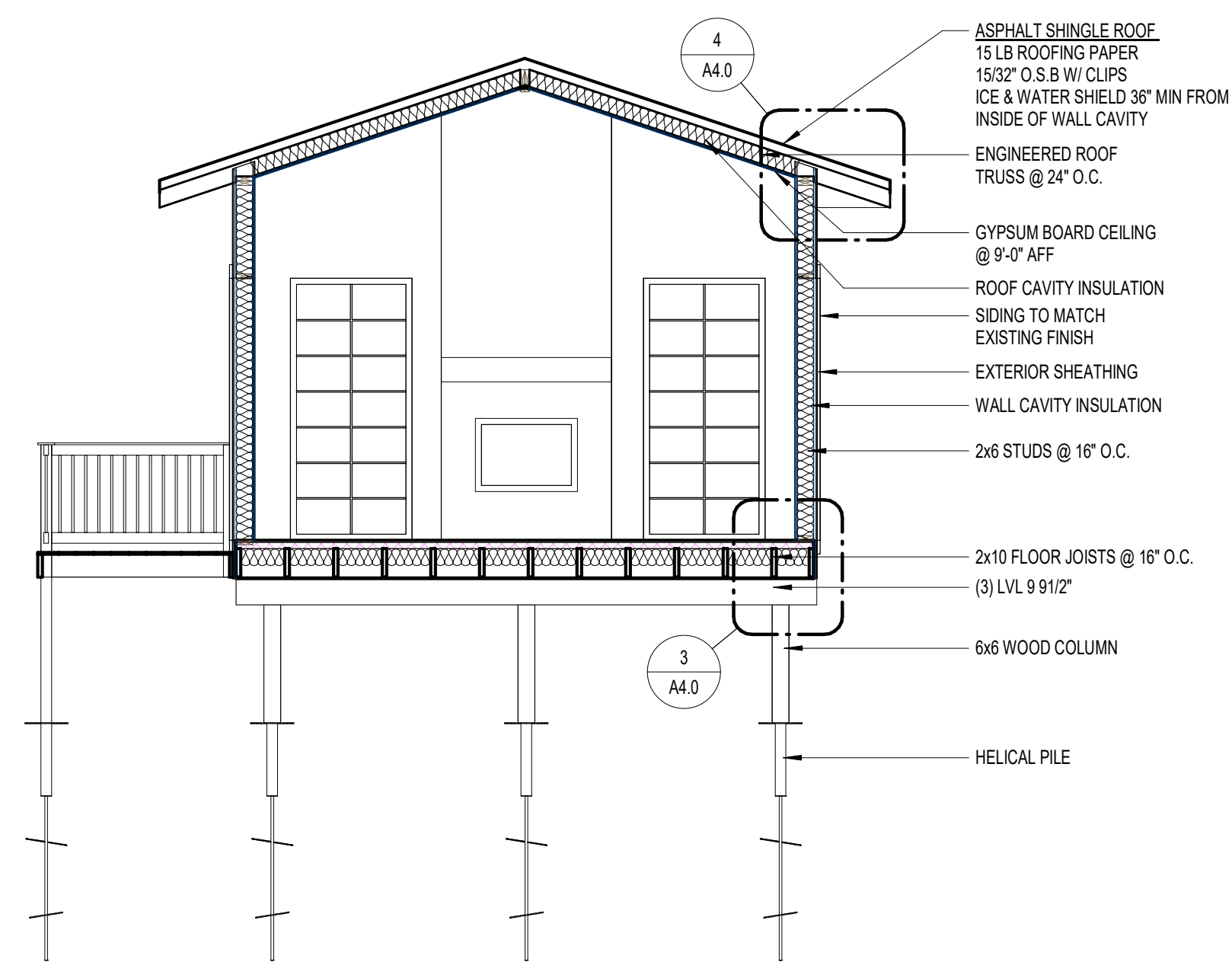
EXTERIOR ELEVATIONS

Project Number 1023-02

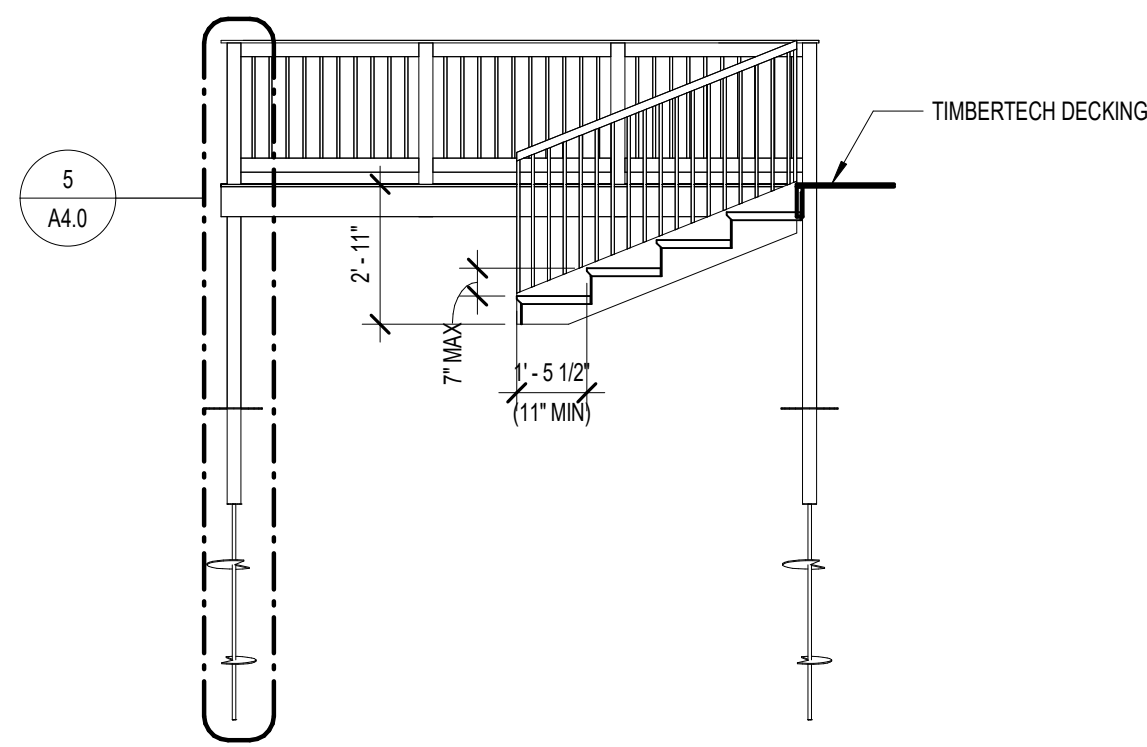
Date 10.03.2025

A3.0

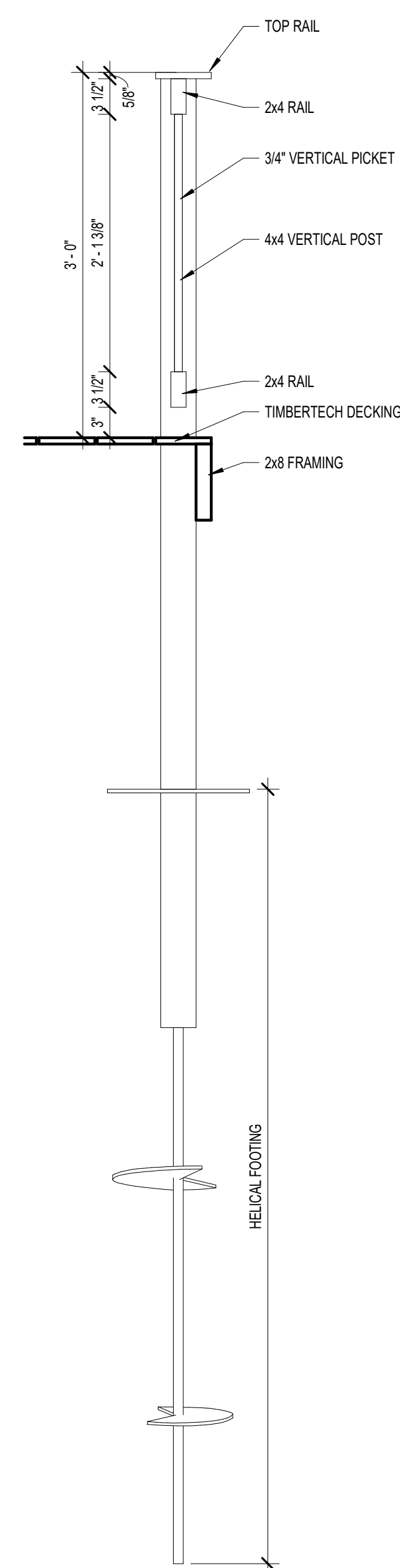
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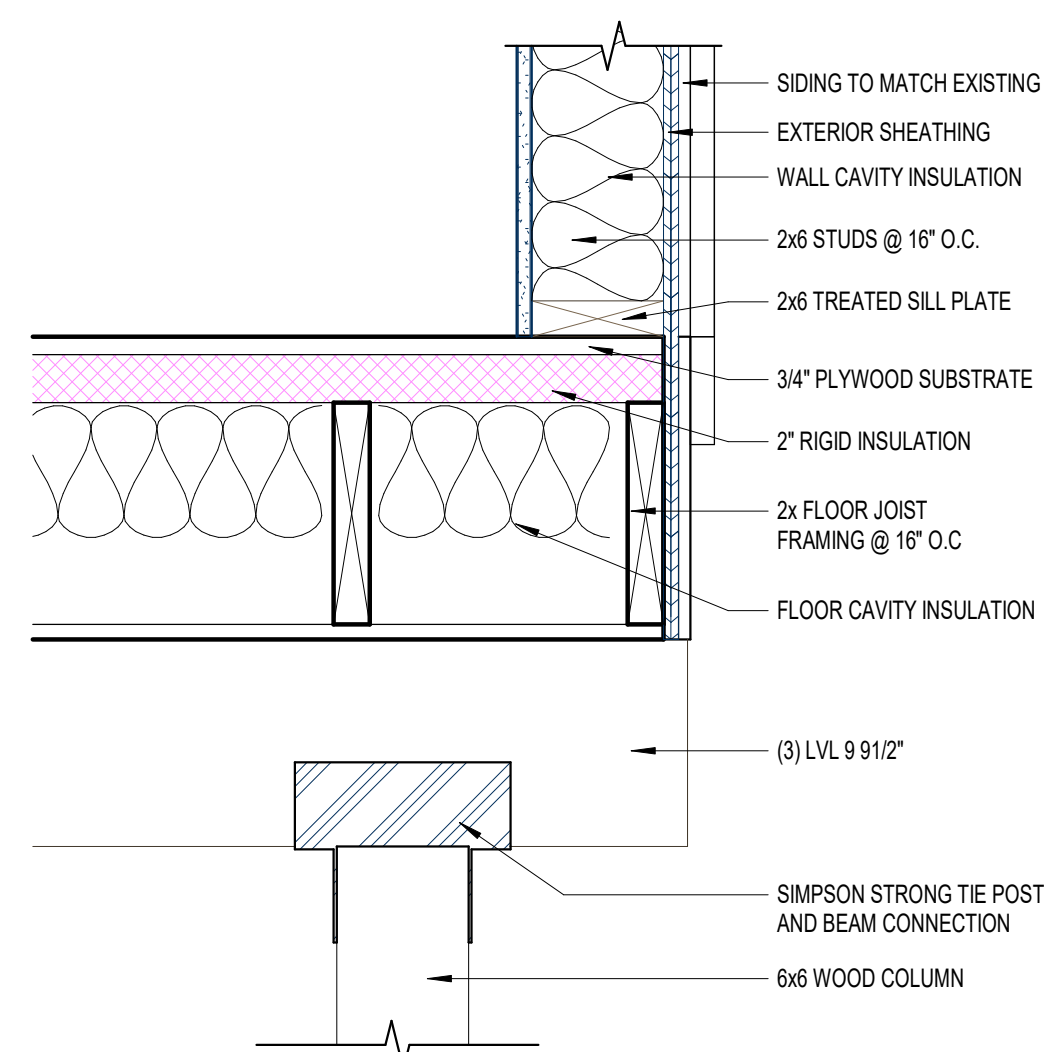
1 SECTION @ EAST - WEST
SCALE: 1/4" = 1'-0"



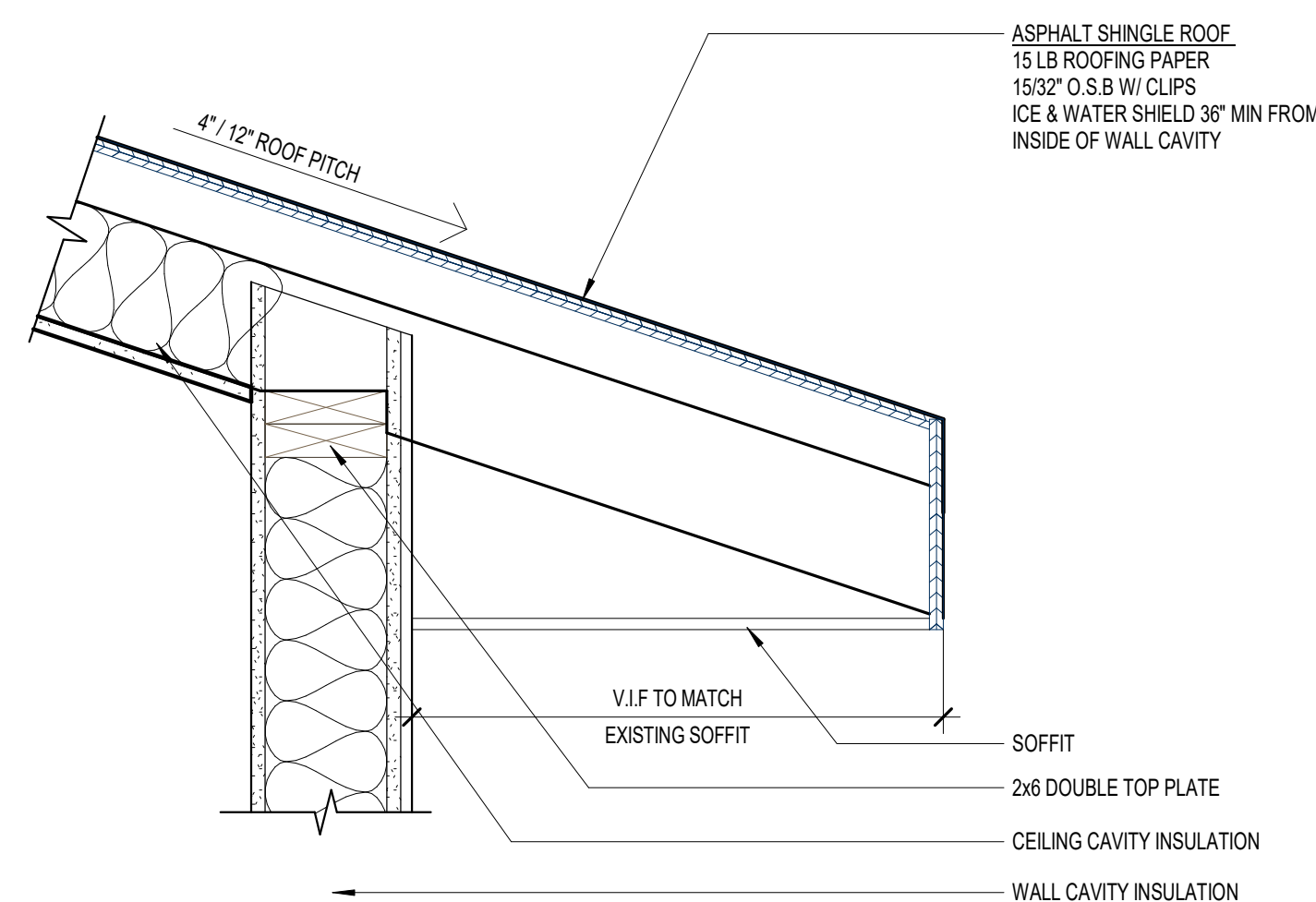
2 SECTION @ DECK STAIRS
SCALE: 1/4" = 1'-0"



5 SECTION AT DECK RAILING AND SUPPORTS
SCALE: 1" = 1'-0"



3 DETAIL @ FOUNDATION/WALL CONNECTION
SCALE: 1 1/2" = 1'-0"



4 DETAIL @ SOFFIT/WALL CONNECTION
SCALE: 1 1/2" = 1'-0"

Hendrickson Residence

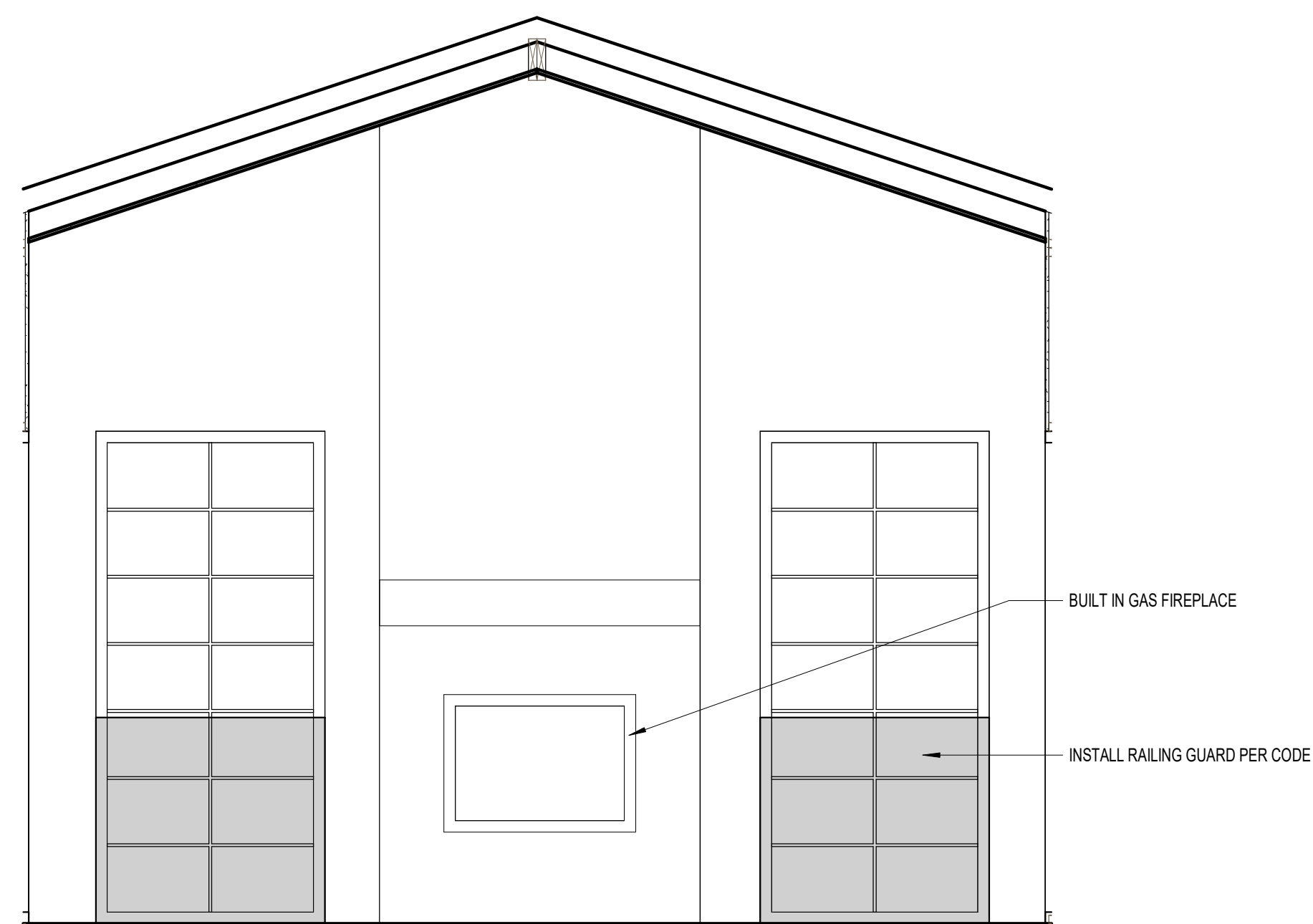
BUILDING SECTION AND EXTERIOR DETAILS

Project Number 1023-02

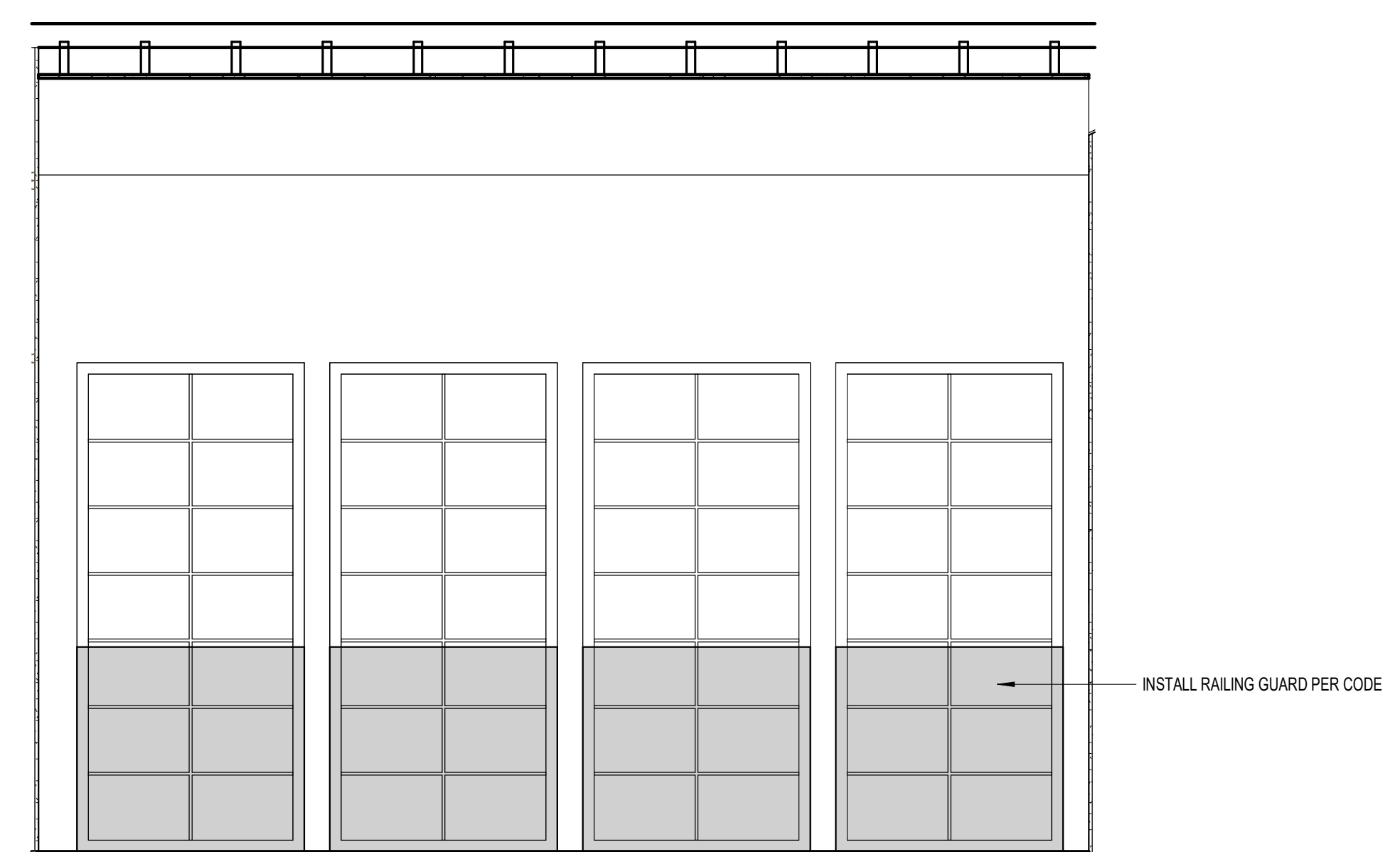
Date 10.03.2025

A4.0

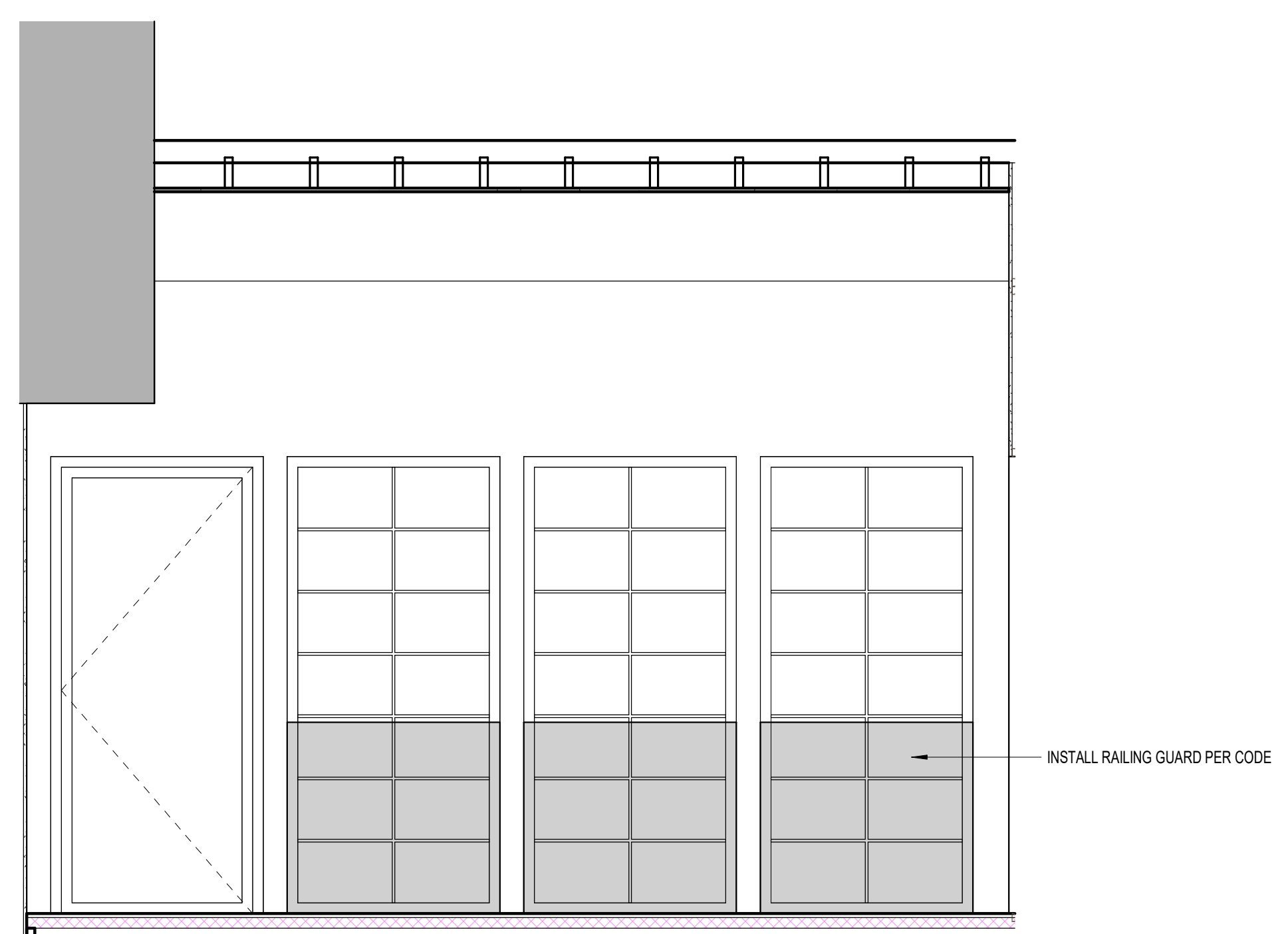
Scale As indicated



1
A5.0 INTERIOR ELEVATION @ FIREPLACE
SCALE: 1/2" = 1'-0"



2
A5.0 INTERIOR ELEVATION @ EAST WINDOWS
SCALE: 1/2" = 1'-0"



3
A5.0 INTERIOR ELEVATION @ EXTERIOR DOOR
SCALE: 1/2" = 1'-0"

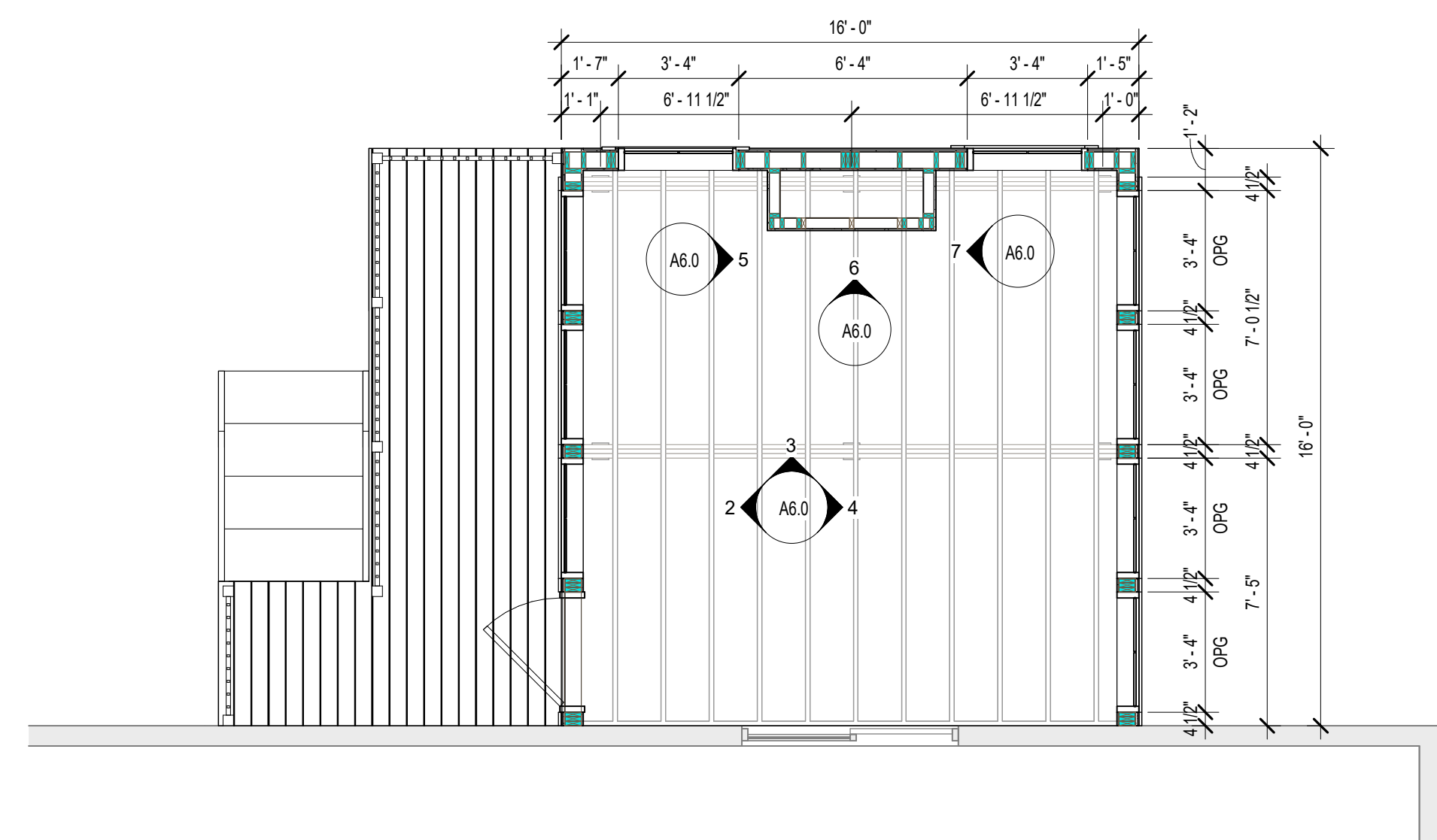
Hendrickson Residence

INTERIOR ELEVATIONS

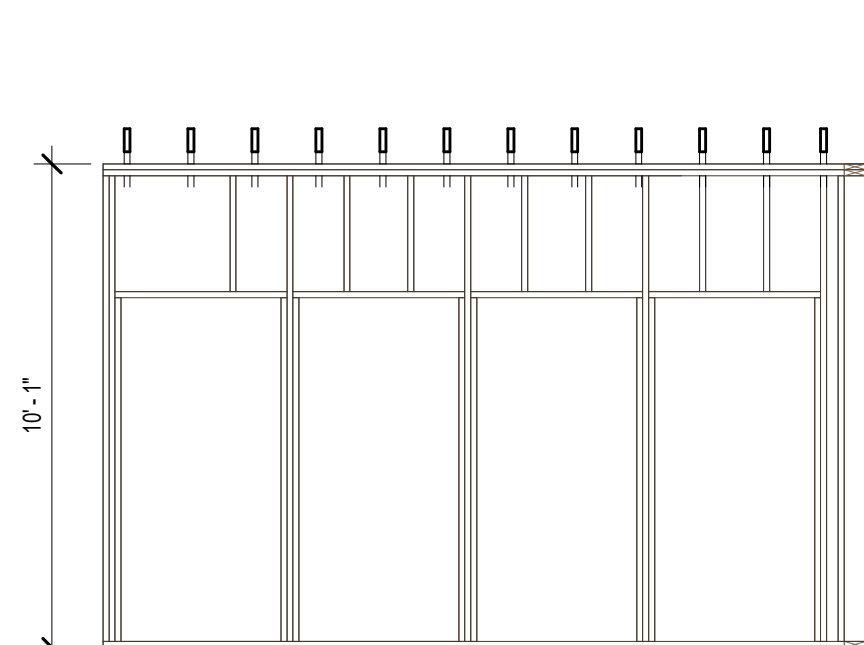
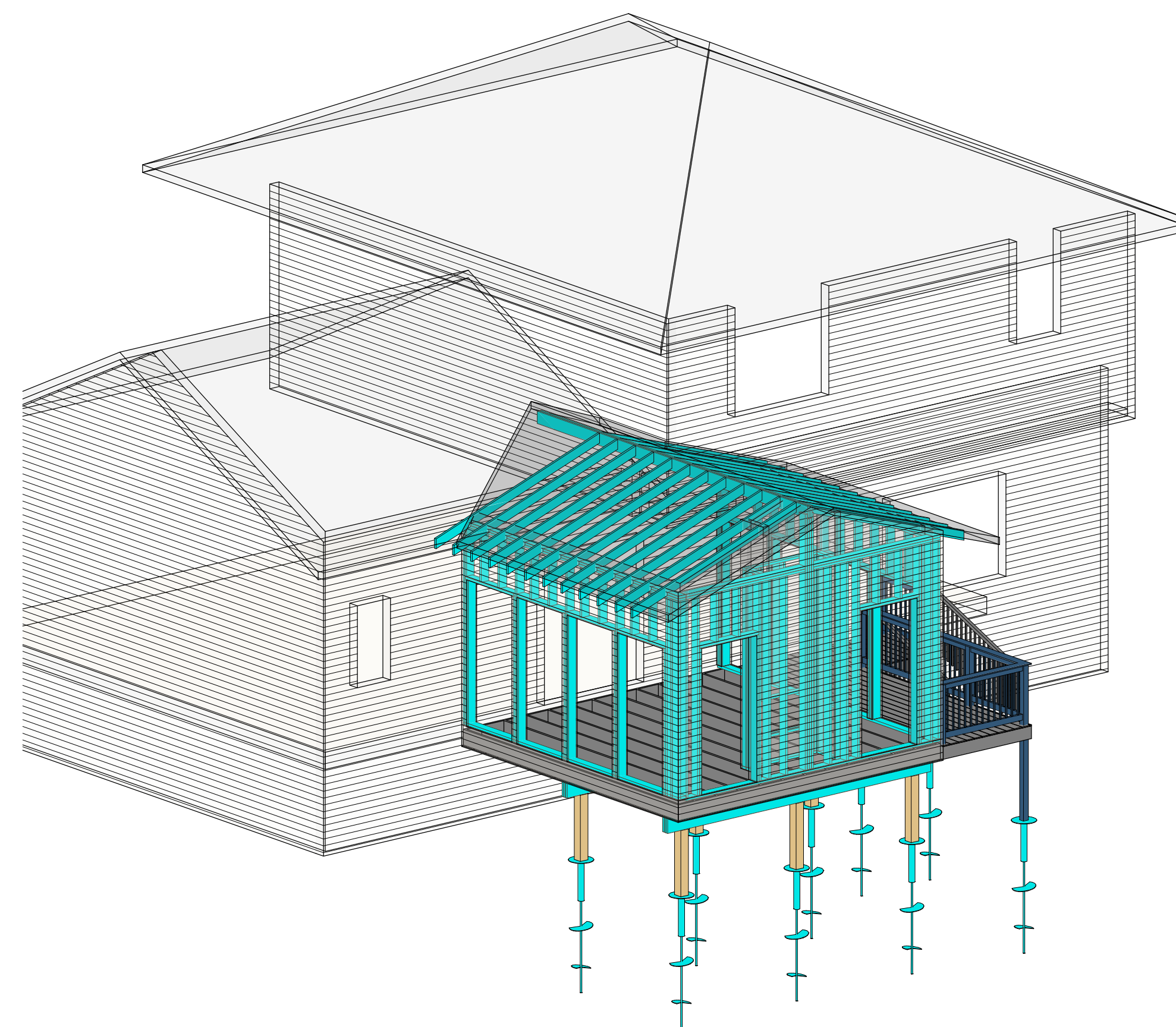
Project Number 1023-02
Date 10.03.2025

A5.0

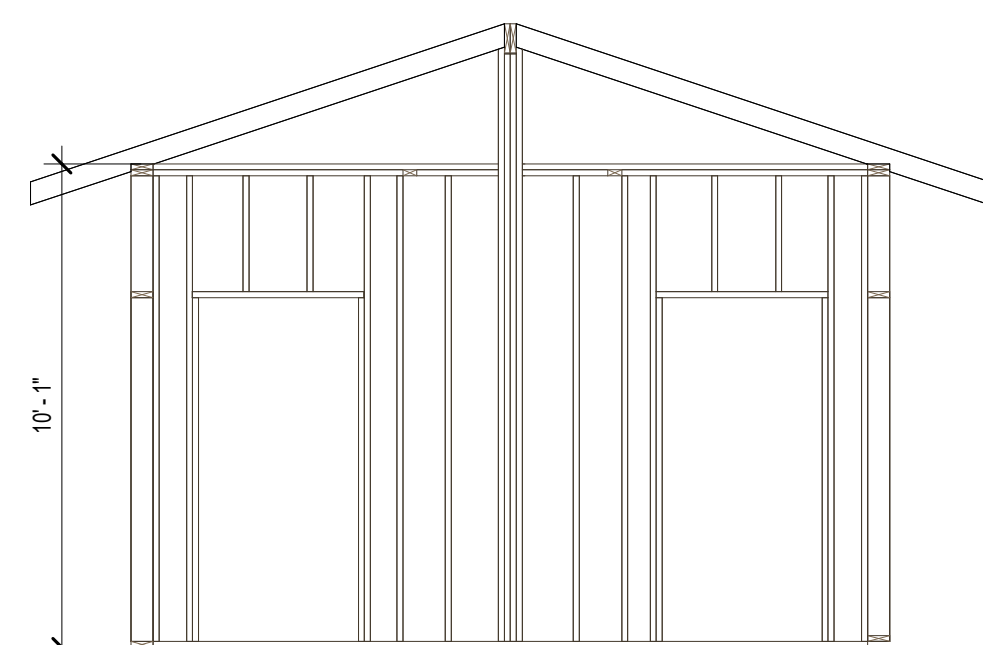
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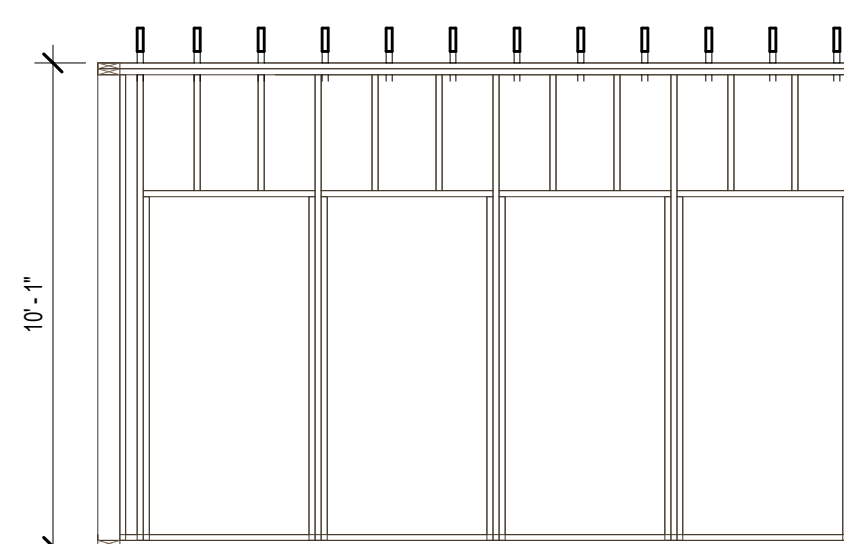
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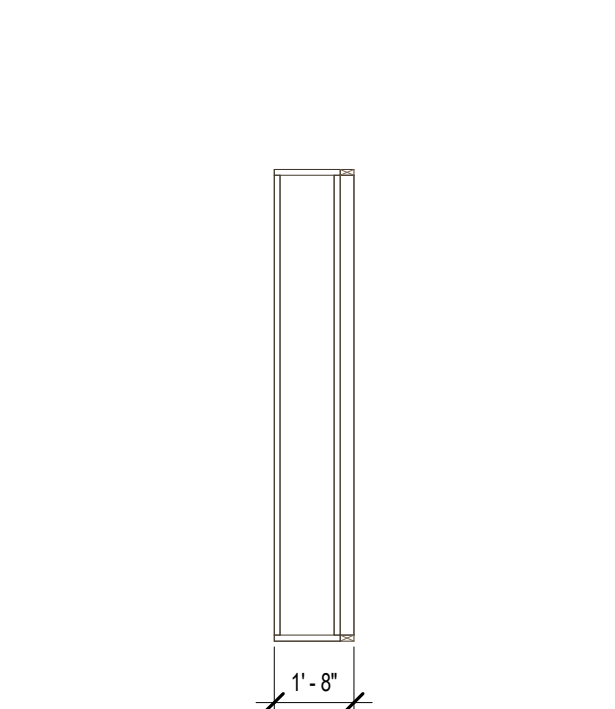
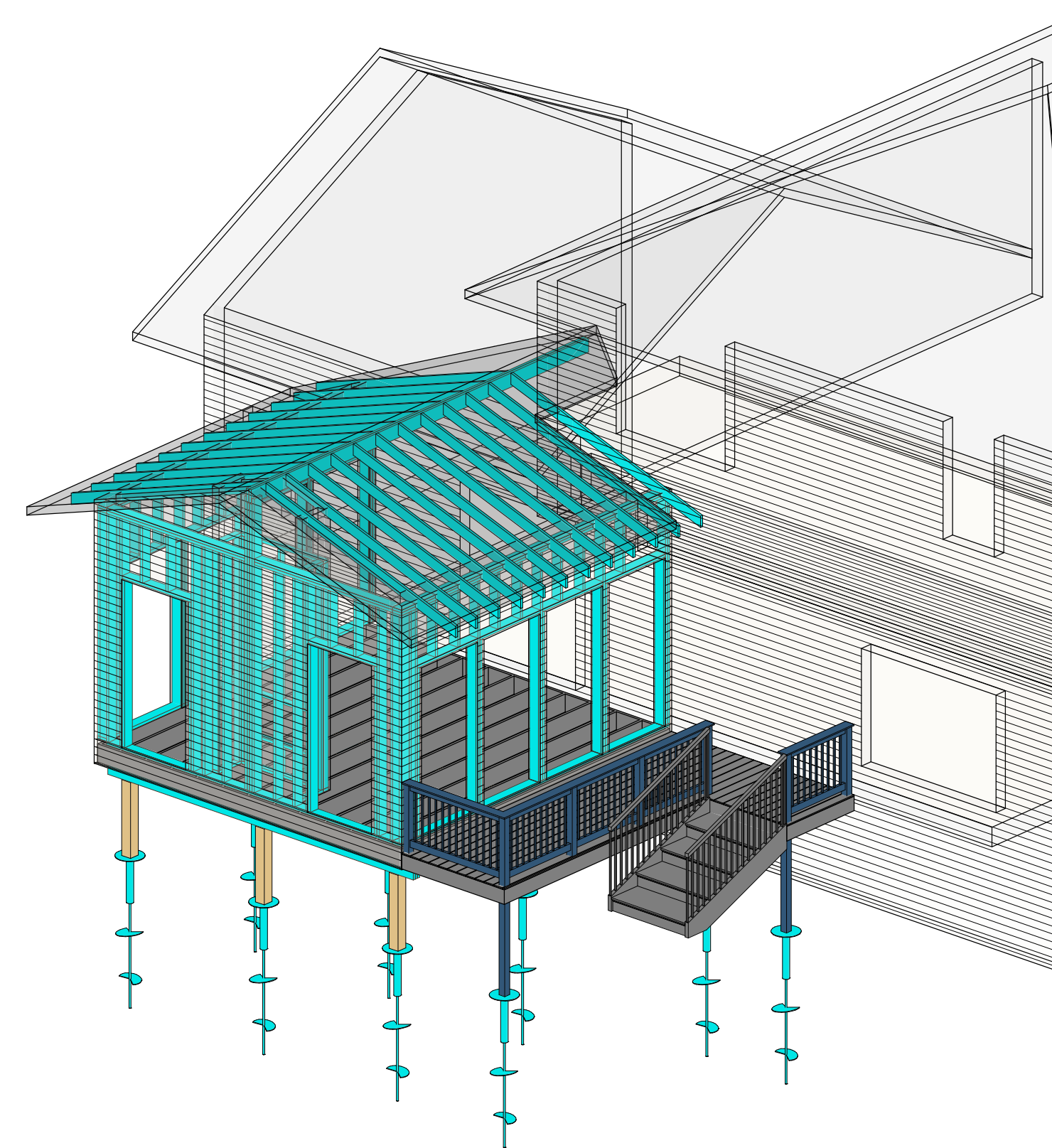
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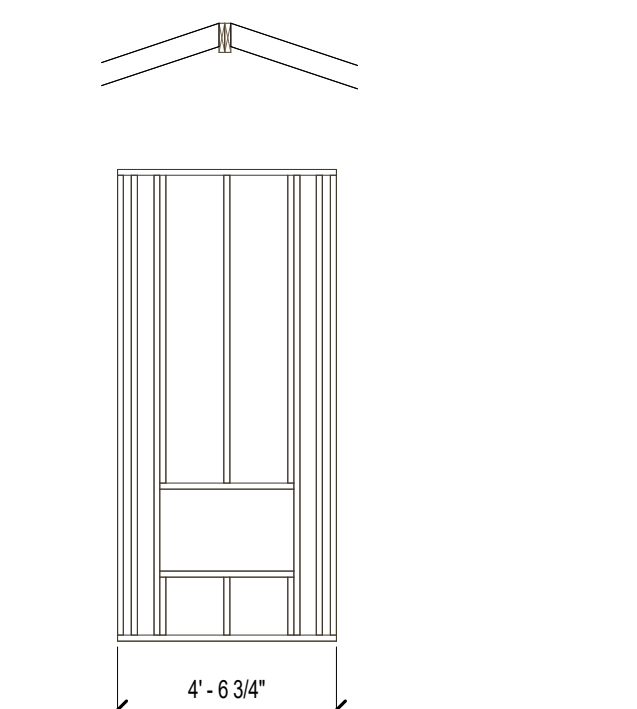
3 FRAMING ELEVATION - NORTH WALL
SCALE: 1/4" = 1'-0"



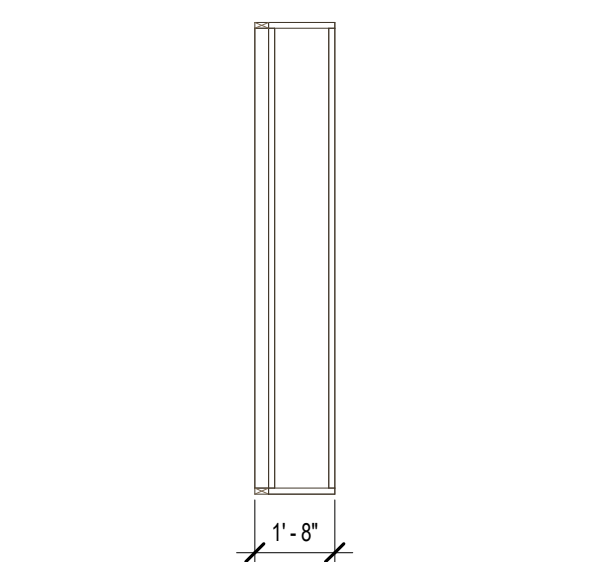
4 FRAMING ELEVATION - EAST WALL
SCALE: 1/4" = 1'-0"



5 FRAMING ELEVATION - FIREPLACE WEST
SCALE: 1/4" = 1'-0"



6 FRAMING ELEVATION - FIREPLACE NORTH
SCALE: 1/4" = 1'-0"



7 FRAMING ELEVATION - FIREPLACE EAST
SCALE: 1/4" = 1'-0"

Hendrickson Residence

FRAMING PLANS

Project Number 1023-02

Date 10.03.2025

A6.0

Scale 1/4" = 1'-0"

Established in 1962
LOT SURVEYS COMPANY, INC.
 LAND SURVEYORS

REGISTERED UNDER THE LAWS OF STATE OF MINNESOTA
 7601 73rd Avenue North
 Minneapolis, Minnesota 55428
 612-660-3093
 Fax No. 660-3622

Surveyors Certificate

T & D HOMES

Property located in Section
 25, Township 31, Range 22,
 Anoka County, Minnesota

INVOICE NO. 47029

F.B.NO. 769-77

SCALE: 1" = 20'

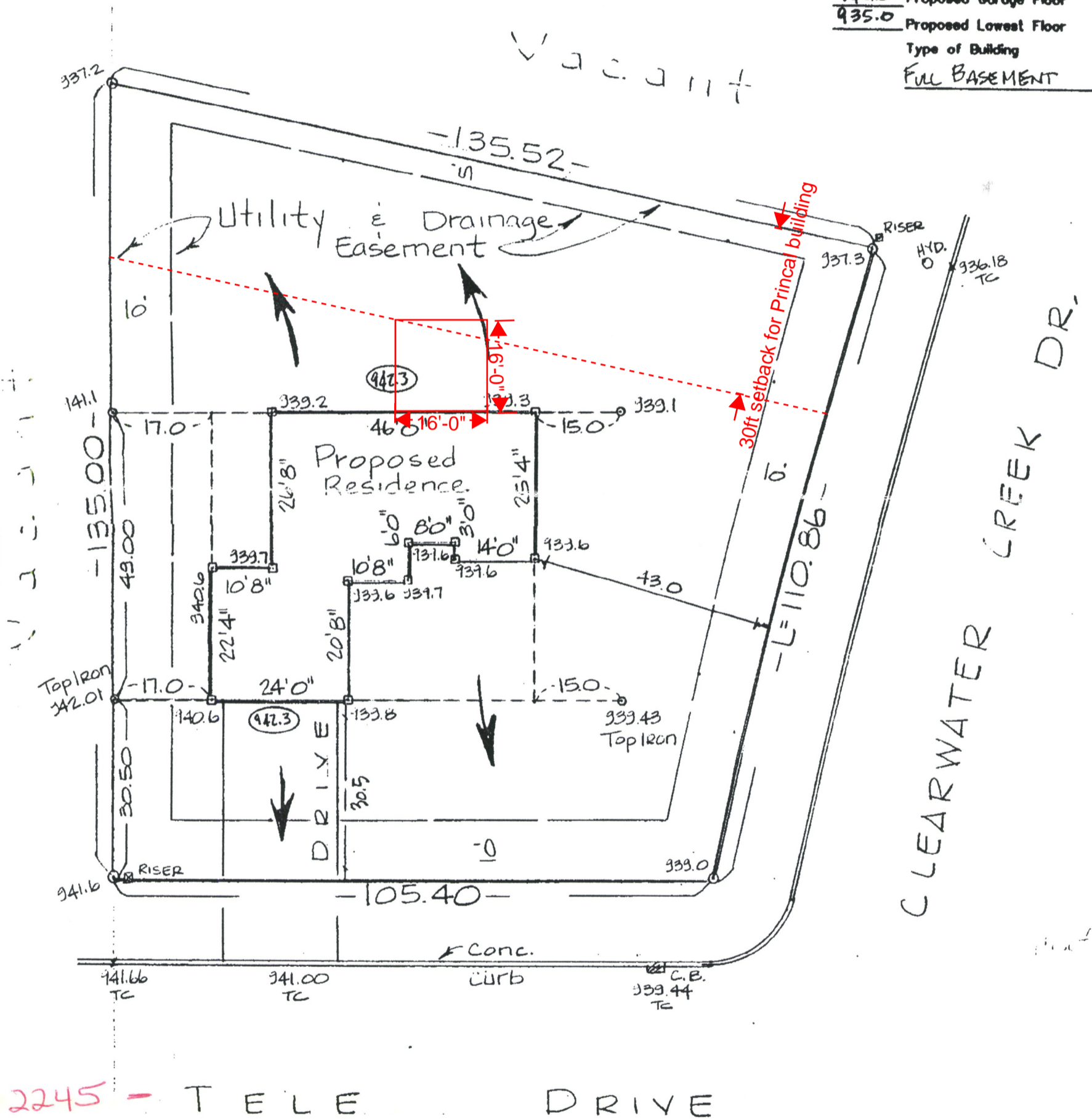
- Denotes Iron Monument
- Denotes Wood Hub Set for excavation only
- x000.0 Denotes Existing Elevation
- 000.0 Denotes Proposed Elevation
- Denotes Surface Drainage

NOTE: Proposed grades are subject to results of soil tests.

Proposed building information must be checked with approved grading plan and development or grading plan before excavation and construction.

943.0 Proposed Top of Block
 942.5 Proposed Garage Floor
 935.0 Proposed Lowest Floor

Type of Building
FULL BASEMENT



Lot 7, Block 2, CLEARWATER CREEK 2ND ADDITION

The only easements shown are from plats of record or information provided by client.

We hereby certify that this is a true and correct representation of a survey of the boundaries of the above described land and the location of all buildings and visible encroachments, if any, from or on said land.

Surveyed by us this 24th day of June 19 97

Signed

Charles F. Anderson

Charles F. Anderson, Minn. Reg. No. 21753

**PLANNING & ZONING BOARD
AGENDA ITEM 6B**

STAFF ORIGINATOR: Katie Larsen, City Planner

MEETING DATE: January 14, 2026

TOPIC: Pine Oaks PUD Concept Plan Review

CASE NUMBER: PZC2025-0010

APPLICANT: M/I Homes of Minneapolis/St. Paul, LLC
Attn: Emily Becker
5354 Parkdale Drive, Suite 100
St. Louis Park, MN 55416

OWNER: Pine Acres Development Co
18183 Lansford Path
Lakeville, MN 55044

INTRODUCTION

The applicant, M/I Homes of Minneapolis/St. Paul, LLC, is proposing 82 single family villa lots in a conservation subdivision. The development is approximately 75 gross acres. It is located northeast of Natures Refuge North and is called Pine Oaks.

Tentative Review Schedule:

Complete Application Date:	December 15, 2025
60-Day Deadline:	February 13, 2026
Environmental Board Meeting:	December 17, 2025
Park Board Meeting:	February 4, 2026
Planning & Zoning Board Meeting:	January 14, 2026
City Council Work Session:	February 2, 2026
City Council Meeting:	N/A

BACKGROUND

The Land Use Application is for the following:

- PUD Concept Plan Review

Future land use applications may include:

- Existing Pine Oaks Plat Vacation
- Rezone property from R, Rural to PUD, Planned Unit Development
- PUD Preliminary Plan/Preliminary Plat
- PUD Final Plan/Final Plat

This staff report is based on the following submittals:

- Applicant Narrative
- Concept Plans prepared by Carlson McCain dated November 19, 2025
- Rare Plant Survey prepared by Midwest Natural Resources dated December 11, 2025

History

Pine Oaks Addition was originally platted in 1980 with 49 lots; however, the development was never constructed. The plat will need to be vacated.

The concept of Natures Refuge, which includes this Pine Oaks area, was first presented to the City in 2006 as a conservation subdivision. The original site was 232 gross acres (approximately 100 acres upland and 132 acres wetlands/conservation easements) and 278 single family units were proposed. A mandatory Environmental Assessment Worksheet (EAW) was prepared because the 278 housing units exceeded the 250 housing unit Environmental Review Record (ERR) threshold. It was determined the project did not have the potential for significant environmental effects and the Council passed Resolution No. 07-06 making a Negative Declaration on the Need for an EIS.

The original Natures Refuge did not materialize past the EAW process due to the decline in the housing market and no preliminary plat was ever submitted. A smaller phase (Natures Refuge) was approved in January 2022 with 60 single family units on 95 gross acres. A second phase (Natures Refuge North) was approved in May 2025 with 94 single family villa lots on 60 gross acres. This last phase, Pine Oaks, proposes 82 villa lots on 75 acres.

	Year	# Lots	Acres
Natures Refuge	2022	60	95
Natures Refuge North	2025	94	60
Pine Oaks	2026	82	75

TOTAL		236	230
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Current Proposal

The proposed Pine Oaks is a continuation of Natures Refuge North with 82 villa homes on 60ft wide lots. The villas provide a life cycle housing option with main-level living and association-maintain yards and snow removal.

Wetlands, buffers, stormwater ponds, and borrow areas are integrated throughout the development creating approximately 55 acres (73%) of open space.

See the attached applicant’s narrative for a detailed description of the project.

ANALYSIS

Existing Conditions

The site contains woodlands, wetlands, and rare plants. It is relatively flat with elevations ranging from 904 to 908 feet mean sea level. A 100ft wide natural gas pipeline easement runs east to west through the site. Nordin Manor to the northeast is a residential subdivision that was platted in 1954.

Surrounding Zoning and Land Use

Direction	Zoning	Current Land Use	2040 Future Land Use
North	City of Columbus	City of Columbus	City of Columbus
South	PUD (Natures Refuge)	Agricultural	LDR, Low Density Residential
East	R-1, Single Family & R, Rural	Rural Residential & Woolan Park	LDR & Park & Open Space
West	R, Rural & PUD	Rural Residential & Single Family Residential	Urban Reserve & LDR

Comprehensive Plan and Housing Density

Planning District #2

The site is located in Planning District 2 which identifies Natural Resources such as Woolan Park. The Comprehensive Plan states:

“The 17 acre Woolan Park Wetland Bank, located north of Main Street and west of Lake Drive, is part of a larger 60 acre wetland complex. Opportunities to protect and restore the balance of the wetland should be evaluated with any adjacent development.”

The proposed development is adjacent to this wetland complex. The site also contains other wetlands and natural resources such as existing vegetation and trees and rare and endangered species. The PUD, Planned Unit Development process provides the opportunity to protect and enhance these public values (natural resources) with a conservation subdivision and development standard flexibility.

Density

Per the City’s 2040 Comprehensive Plan the site is guided for low density sewer residential development which requires 1.6 to 3.0 units per net acre. The proposed net density is 2.76 units per acre which is consistent with the Comprehensive Plan’s guided land use and density range.

The following chart implements Met Council’s formula for calculating net density.

Gross Area (acres)	75.42
Wetlands & Water Bodies	(31.02)
Public Parks & Open Space	0.00
Arterial ROW	0.00
Other (Utility Transmission Easement)	(0.37)
Other (Wetland Buffer Area)	(14.37)
Net Area (acres)	29.66
# of Units	82
Gross Density (units/acre)	1.09
Net Density (units/acre)	2.76

Zoning and Land Use

Current Zoning	R-1, Single Family Residential
Current Land Use	Vacant Residential
Future Land Use per 2040 Comp Plan	Low Density Residential
Utility Staging Area	Stage 1B (2025-2030)

Zoning Requirements and Standards

The site is currently zoned R-1, Single Family Residential. The property will be rezoned to PUD, Planned Unit Development, the same as Natures Refuge and Natures Refuge North. A summary of PUD public benefit vs PUD flexibility is provided at the end of this staff report.

Conventional Zoning Requirements vs. PUD Guidelines

Per City Code Section 1007.024(4) for PUD, Planned Unit Development, the development guidelines for land guided low density residential development are either R-1, R-1X, or R-2.

The following chart is from the applicant’s narrative. The R-2, Two Family Residential will be the underlying conventional zoning district.

Standard	R-2 Zoning Standard	Villa Lots
Minimum Lot Size	7,500 square feet	7,500 square feet
Minimum Lot Width	60 feet	60 feet
Minimum Lot Depth	125 feet	125 feet
Minimum Front Setback	25 feet	25 feet
Minimum Side Setback	10 feet (5 feet for Attached Garage)	7.5 feet*
Minimum Corner Side Setback	25 feet	20 feet*
Rear Setback	25 feet	25 feet
Impervious Surface	65%	65%
Collector Road Right-of-Way Width	80 feet	60 feet*
Garage Size	Minimum 20 feet in width and 520 square feet in size	We agree to adhere to these requirements.
Garage Design	Garage shall not protrude forward of the front wall of the house, unless the house includes a front porch that is unenclosed, a minimum of six (6) feet in depth, and extends across 100 percent of the front façade of the house. The garage shall then not protrude forward of the front porch.	We are requesting this be waived but agree that the garage cannot protrude more than five feet forward the house.*

Garage Design	The length of the garage wall facing the street shall not be greater than 50 percent of the length of the entire front façade of the structure for street-facing garage doors	Garage walls shall not be greater than 57 percent of the length of the entire front façade.*
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**PUD flexibility required.*

Staff Comment:

1. Per the subdivision ordinance, minimum lot area for urban lots shall consist of buildable land exclusive of utility transmission easements or water course easements that encumber lot development.
 - a. Lots shall not contain rare plants, wetlands, or wetland buffers .
 - b. Lots that contain the powerline easement shall demonstrate they meet the 7,500sf buildable land area requirement.
2. Similar to Natures Refuge North, the following PUD flexibility is proposed:
 - a. Garage shall be allowed to protrude forward of the house, without front porch, a distance of no more than 5', subject to conformance with an anti-monotony plan that prohibits houses with protruding garages to be placed directly adjacent to or across from one another.
 - b. Unless 2nd floor living space extends over the garage, the maximum length of the garage wall facing the street shall not exceed 57% of the front façade subject to the following:
 - i. Decorative garage doors with a strip of windows are required.
 - ii. Garage width in excess of two stalls will be recessed two feet from the main garage facade.
 - iii. Total garage width will not exceed 32 feet.
3. The Net Area Exhibit shall include the lot numbers.

Subdivision Ordinance

Blocks and Lots

The proposed development provides 82 single family villa lots and several outlots that will contain wetlands, buffers, stormwater ponds etc.

Streets and Alleys

Street A will connect to Pine Street and the Orange Street area. Street B is a continuation of Haywood Drive. It is identified as a minor collector road in the Transportation Plan and provides a north-south corridor from CSAH 14 (Main Street) to

eventually Pine Street. All proposed street right-of-way widths are 60ft and the paved surface is 32ft wide.

Street connections are required by the following City Code Sections:

§ 1001.096 LAND REQUIREMENTS.

(2) Proposed subdivisions shall be coordinated with surrounding properties and/or neighborhoods so that the city as a whole may develop efficiently and harmoniously.

§ 1001.100 STREETS AND ALLEYS.

(3) *Streets, continuous.* Except for cul-de-sacs, streets shall connect with streets already dedicated in adjoining or adjacent subdivisions or provide for future connections to adjoining unsubdivided tracts, or shall be a reasonable projection of streets in the nearest subdivided tracts. The arrangement of thoroughfares and collector streets shall be considered in their relation to the reasonable circulation of traffic, to topographic conditions, to runoff of storm water, to public convenience and safety, and in their appropriate relation to the proposed uses of the area to be served.

Staff Comments:

1. A traffic study shall be required.
2. Trails and sidewalks shall be incorporated throughout the development.
3. 40ft of road right-of-way shall be dedicated along Pine Street.
4. Due to wetlands within the Nordin Street road right-of-way, further analysis will be required to determine if a street will be constructed to Pine Street near Olive Street and Orange Street.
5. It appears that a portion of Street A connecting to Nordin Street road right-of-way is outside of the Pine Oaks Addition. This shall be evaluated.
6. Lot 82 will require vacating a portion of the Nordin Street road right-of-way. This shall be evaluated.

Easements

Staff Comments:

1. Standard drainage and utility easements at least 10 feet wide shall be provided along all lot lines.
2. Drainage and utility easements shall also be dedicated over stormwater management facilities.
3. Oversized easements for stormwater management may not be included in the minimum established lot size.
4. Conservation easements and wetland buffers shall be required over wetlands and greenway corridors.

Stormwater Management and Erosion and Sediment Control

The concept plan shows several stormwater management facilities, ponds, and wetlands throughout the development creating a multi-functional greenway corridor.

Utilities

Public water, sanitary, and storm sewer utilities will be installed within the development. Water and sanitary will extend north along Haywood Drive.

The site is located in Sanitary Sewer District 2 (Sub-district 2K). Flow is collected southward to MCES Meter M211 and Gravity Interceptor 8361 along North Road which passes southwest through Circle Pines.

The development is located in Utility Staging Area 1B (2025-2030). By resolution, the Council will open this staging area.

Municipal sanitary sewer and water also gets extended closer the Olive and Orange Street neighborhood which are smaller (<0.5 acre) lots on private septic systems and wells.

Parks, Greenways and Trail Plan

Per the City's 2040 Parks, Greenways and Trails Plan, this development is within the Arena Acres and Wollan Park service areas. Arena Acres is a smaller 1 acre neighborhood park. Wollan Park is an 18 acre open space wetland and not an active park. Lino Lakes Elementary School also serves as the neighborhood park.

Staff Comments:

1. Trail locations shall be evaluated to extend from the existing trail in Natures Refuge North to Wollan Park.

Tree Preservation and Landscaping

A Tree Preservation Plan and Landscape Plan shall be submitted with the PUD Preliminary Plan/Preliminary Plat submittal. In lieu of mitigation for each tree the developer is proposing 3 trees per lot, restoration of existing protected upland, and dedication of land and open space for future management. Additional landscaping may be required as a condition of the PUD.

Traditional/Conventional Development vs Conservation Development

A traditional or conventional development uses the standard zoning district requirements that can result in a street grid development.

Per City Code Section 1007.007: Subdivision Ordinance Definitions

CONSERVATION DEVELOPMENT. The development pattern and technique whereby lots are arranged in closely related groups to preserve the natural amenities of the land through the creation of common open space.

Per City Code Section 1001.026:

(4) Yield plan. If the project includes a conservation subdivision or planned unit development, the applicant shall submit a yield plan showing the maximum number of dwelling units that would be permitted given the minimum lot size and lot widths for conventional subdivisions and other requirements of the Lino Lakes Zoning and Subdivision Chapters.

A conservation development is the same as a conservation subdivision.

Staff Comment:

1. The applicant shall submit a yield plan with the PUD Preliminary Plan/Preliminary Plat submittal.

The purpose of using the PUD zoning is to create a conservation development that establishes the defined public values of preserving and enhancing the on-site natural resources. The PUD also creates and enhances the greenway corridor and trails that are accessible to the public.

Environmental Review Record

As previously noted, in 2006 a mandatory Environmental Assessment Worksheet (EAW) was prepared for the original Natures Refuge concept because the proposed 278 housing units exceeded the 250 housing unit threshold. The EAW thoroughly analyzed potential impacts to wetlands, rare plant and animal species, floodplain, water quality, noise, traffic and archeology. It was determined the project did not have the potential for significant environmental effects and the Council passed Resolution No. 07-06 making a Negative Declaration on the Need for an EIS.

The scope and size of the existing Natures Refuge, Natures Refuge North, and proposed Pine Oaks is smaller than the original 2006 proposal. A revised EAW is not required because the 236 housing units do not exceed the 250 housing unit threshold.

	# Housing Units
Natures Refuge	60
Natures Refuge North	94
Pine Oaks	82
TOTAL	236

Wetlands

Per the December 17, 2025 Environmental Board staff report:

Eight (8) wetlands were delineated onsite totaling 34.55 acres. Wetland 8 is an extension of the Wollans Wetland. Extended buffers and upland should be provided between proposed houses and wetland. A minimum of 50’ should be provided for Wetland 8; no buffers should be included in the proposed lots. Avoidance of impacts to large wetlands should be encouraged with the development. Proposed road will cut through a portion of Wetland 3, ensure that hydrology is not impacted and the overall wetland function is maintained in both lobes.

The proposed project areas fall within the RCWD Wetland Management Corridor (WMC). Confirm that all buffers and management meet RCWD requirements within the WMC.

Staff Comments:

1. There shall be more separation and buffer between Lots 56-66 and the wetland.
2. No wetlands or wetland buffers shall encroach into any lot.

Rare Plant Survey

A Rare Plant Survey was prepared by MNR. Three (3) state-threatened species and one (1) endangered species were detected on site. The proposed Pine Oaks layout avoids these rare species.

Staff Comments:

1. Lot 12 shall be evaluated for proximity and potential impacts to the rare plants.

Floodplain

Per the December 17, 2025 Environmental Board staff report:

Floodplain - Floodplain is shown on the south and east edges of the site. Any impacts to the floodplain will need to be mitigated for. A Letter of Map Revision will need to be completed for any lots that are shown within the FEMA floodplain. Coordinate with RCWD on impacts within the floodplain.

Shoreland District

The site is not located within the Shoreland Management Overlay District.

Additional City and Government Agency Review Comments

- The Police Department reviewed the concept plan and had no objections or suggestions.
- The Fire Department received the concept plan and had no comments.
- The Environmental Board reviewed the PUD concept plan on December 17, 2025. There was general support for the project. See attached Environmental Memo for detailed comments.
- The Park Board will review the concept plan February 2026. We anticipate the Board will recommend a 100% cash dedication in lieu of dedicating land for a neighborhood park. The cash dedication could be utilized for trails, providing leisure opportunities at Lino Lakes Elementary, and making enhancements to Arena Acres Park and Wollan Park.
- The City Engineer provided comments in the attached memo.

PUD Preliminary Plan/Preliminary Plat

The City's review and comments on the concept plan's relation to the Comprehensive Plan and development regulations does not grant any rights to the applicant to develop the property as depicted by the concept plan.

A land use application for PUD Preliminary Plan/Preliminary Plat shall be required. The developer should have a neighborhood meeting prior to submittal of a PUD Preliminary Plan/Preliminary Plat.

PUD Public Benefit vs PUD Flexibility Summary

Per City Code Section 1007.024:

(2) The PUD, by allowing deviation from the strict provisions of this ordinance related to setbacks, heights, lot area, width and depths, yards, etc., may be considered by the City when it would result in one or more of the following public benefits:

- (a) Implementation of a master plan consistent with the Planning District objectives of the Comprehensive Plan.

The need for a formal master plan was not identified for this area in the 2040 Comprehensive Plan. The proposed development is consistent with the 2006 EAW concept plan for low density single family residential conservation development.

- (b) Innovations in development that address growing demands for all styles of economic expansion, greater variety in lot size, configuration, home type, design, enhanced architectural standards, and siting of structures through the conservation and more efficient use of land in such developments.

The applicant is proposing a conservation development. The villas provide a housing option with main-level living with association-maintain yards and snow removal which addresses life-cycle housing. The project will include enhanced architectural design requirements and an anti-monotony code.

- (c) Preservation and enhancement of desirable site characteristics such as wildlife habitat, unique natural resources, existing vegetation, natural topography, geologic features and reduction of negative impacts on the environment.

The conservation development preserves and enhances desirable site characteristics by minimizing wetland, woodland, and rare plant species impacts. The site will include approximately 55 acres (73%) of open space preserved in a conservation easement.

- (d) Creative use of land and related physical development which allows a phased and orderly transition of varying land uses in close proximity to each other.

Pine Oaks is a continuation of Natures Refuge North conservation subdivision.

- (e) Efficient use of land resulting in smaller networks of utilities and streets thereby lowering development costs and public investments.

The configuration of the lots preserves open space and as many wetlands as possible and avoids a gas line easement that spans the site diagonally which results in a smaller network of utilities and streets.

- (f) Mix of land use types.

There is no mix of land use types.

- (g) Provision of a housing type or target housing price that is desirable to the City.

The villas provide a housing option with main-level living with association-maintain yards and snow removal which addresses life-cycle housing.

- (h) Other public benefits and values as recognized in the City's Comprehensive Plan.

- *Planning District 2 identifies Wollan Park as a natural resource that should be protected. The proposed development strategically includes a stormwater pond/open space buffer near the Wollan Park wetland complex. This is a critical link to protecting this sensitive natural resource area.*
- *A goal of the sanitary sewer plan is to provide service and capacity to developed areas of the city currently served by on-site systems. Municipal sanitary sewer and water also gets extended north which will eventually serve the Olive and Orange Street neighborhood which are smaller (<0.5 acre) lots on private septic systems and wells.*
- *A goal of the Parks, Greenways & Trails Plan is to establish greenway corridors. A multi-functional greenway corridor incorporating stormwater management, trails and open space will be evaluated.*
- *The project will include a stormwater reuse irrigation system reducing demand on the City's water supply system.*

Summary

Public Benefit Gained:

1. Conservation development that protects and enhances the on-site natural resources such as wetlands, woodlands, rare plant species, and open space that are accessible to the general public.
2. Main-level living villa homes with association-maintain yards and snow removal which addresses life-cycle housing.
3. Preserved wetlands and established wetland buffers/conservation easements
4. Efficient use of land resulting in smaller network of utilities and streets
5. Protection of Wollan Park wetland complex.
6. Extension of sanitary sewer to eventually serve Olive and Orange Street neighborhood currently served by on-site systems.
7. Establish multi-functional greenway corridor and trail system that is accessible to the public.
8. Cash fee in lieu of land dedication will be collected in addition to the open space provided in conservation easements or City ownership.

Flexibility Provided:

1. Reduced minor collector road right-of-way width from 80 feet to 60 feet on Haywood Drive (Street A).
2. Reduced corner lot width from 80 feet to 70 feet.
3. Reduced setback from street (corner lot) from 25 ft to 20 feet.
4. Side yard setbacks from 10ft/5ft to 7.5 feet.
5. Garage allowed to protrude forward of the house, without front porch, a distance of no more than 5', subject to conformance with an anti-monotony plan that prohibits houses with protruding garages to be placed directly adjacent to or across from one another.
6. Unless 2nd floor living space extends over the garage, the maximum length of the garage wall facing the street shall not exceed 57% of the front façade subject to the following:
 - (a) Decorative garage doors with a strip of windows are required.
 - (b) Garage width in excess of two stalls will be recessed two feet from the main garage facade.
 - (c) Total garage width will not exceed 32 feet.
7. Tree preservation requirements offset by the following:
 - a. Developer shall grant the City a conservation easement over outlots.
 - b. Developer will plant 3 trees per lot in addition to open space plantings around storm water management areas.

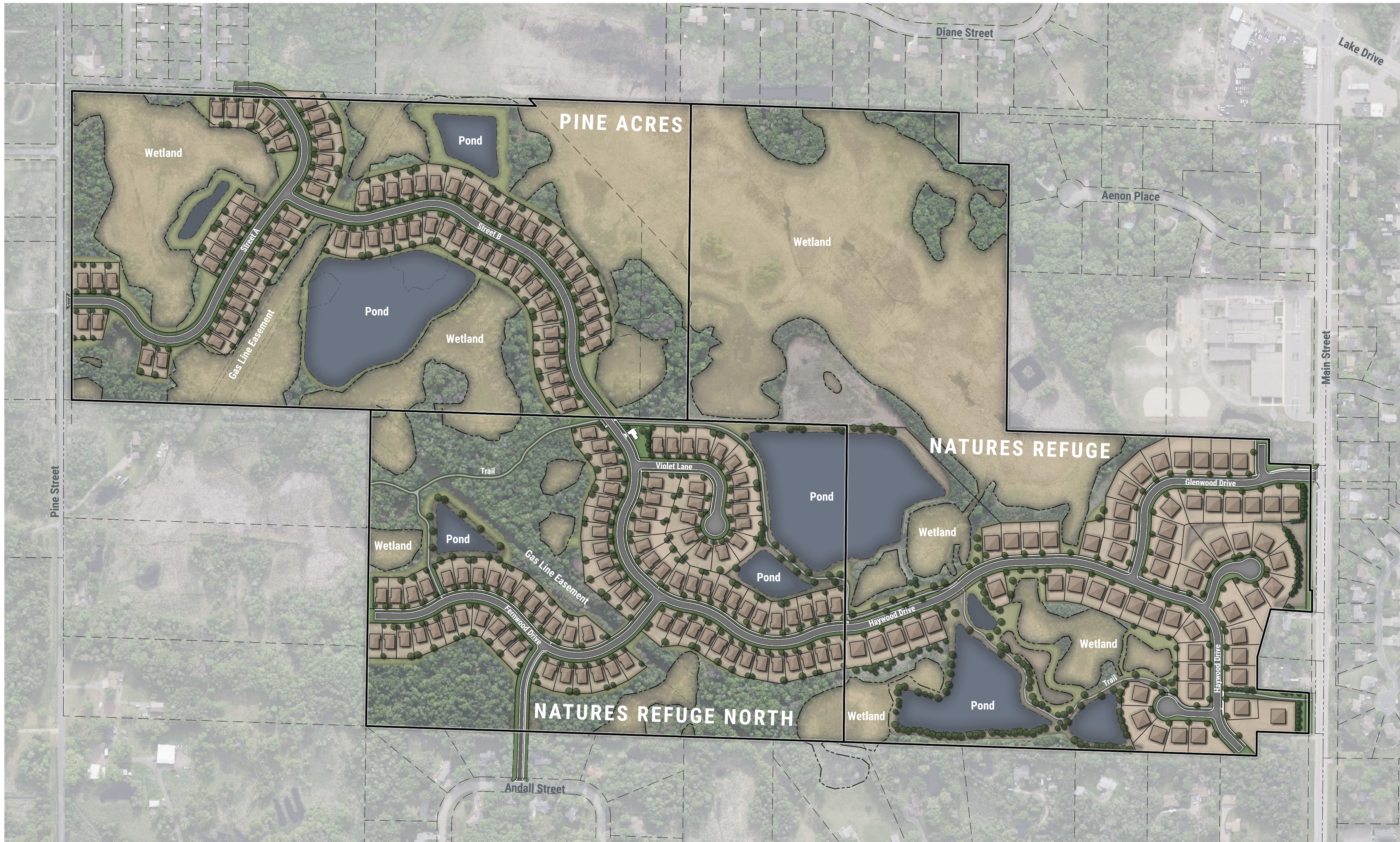
RECOMMENDATION

Staff is requesting feedback from the Planning & Zoning Board:

1. Thoughts on PUD flexibility?
2. Should the applicant hold a neighborhood meeting prior to PUD Preliminary Plan/Preliminary Plat submittal.

ATTACHMENTS

1. Site Location and Overall Neighborhood Plan
2. Applicant Narrative
3. Concept Plan with Aerial
4. Concept Plan with Site Data
5. Concept Plan with Wetlands
6. Concept Plan with Rare Plants
7. City Engineer Memo dated January 7, 2026
8. Environmental Coordinator Memo dated December 17, 2025



**CONCEPT PLAN
REVIEW
REQUEST
PINE OAKS**

INTRODUCTION

M/I Homes of Minneapolis/St. Paul, LLC (M/I Homes) is submitting an application for a concept plan for a Planned Unit Development (PUD) for land to the northeast of Natures Refuge North. The concept plan proposes 82 lots on an approximately 75.42-acre site.

ABOUT M/I HOMES

M/I Homes has been in business for almost 50 years and has built over 150,000 homes in 17 markets within the United States. We entered the Twin Cities Market in 2015 by purchasing Hans Hagen Homes. Hans Hagen Homes was founded over 50 years ago and was one of the largest and oldest home builders in the Minneapolis market.

HISTORY OF DEVELOPMENT/REASON FOR STREET VACATION

The property was originally platted into 49 lots in 1980 but was never developed and has remained vacant and heavily wooded with wetlands. The layout of the existing plat shows little regard for wetland impacts, as a number of lots are mostly or totally encumbered by wetlands, and so the plan is virtually undevelopable with current rules that have been adopted since the plat was recorded. As such, we will be requesting vacation of the streets and easements on the plat with submittal of the preliminary plat and Planned Unit Development (PUD) application. This would result in 49 lots with vacated streets, which would be referenced with a new plat dedication. We will dedicate easements over existing streets so that access to the parcel may be maintained.

PLANNED UNIT DEVELOPMENT

We have seen a great demand for villa homes on smaller lots with main-level living with association-maintained yards and snow removal with our Natures Refuge North development. As such, we are proposing the same type of home in this development and requesting similar flexibilities.

The City of Lino Lakes allows for development through a PUD if it results in public benefit as outlined in the City's PUD ordinance. We believe this proposed development provides the following public benefits:

- *Addressing growing demands for all styles of economic expansion, greater variety in lot size, configuration, home type, design, enhanced architectural standards, and siting of structures through the conservation and more efficient use of land in such developments.*
- *Preservation and enhancement of desirable site characteristics such as wildlife habitat, unique natural resources, existing vegetation, natural topography, geologic features and reduction of negative impacts on the environment.*
- *Provision of a housing type or target housing price that is desirable to the city.*
 - The main-level villa homes that have association-maintained yards and snow removal are popular with our senior buyers or anyone looking for a more carefree living lifestyle. This type of housing is not commonly found in the City of Lino Lakes. The smaller lots make it possible to offer association-maintained yards, as it is not viable for an association to maintain a larger lot. The configuration of the lots, as well as keeping the homes all slab-on-grade, works to preserve as many wetlands, trees and rare plants as possible and avoids a gas line easement that spans the site diagonally.
- *Other public benefits and values as recognized in the city's Comprehensive Plan.*
 - The first goal outlined in the City's Sanitary Sewer Chapter of its Comprehensive Plan is to serve the city's residents and businesses with an affordable and safe sanitary sewer. A policy developed from this goal is to provide for the capacity and extension of sanitary sewer to

developed areas of the city currently served by on-site systems. The proposed development will provide sewer connectivity to the properties to the east located on Olive and Orange Streets and a portion of Pine and Maple Streets that are in the Stage 1B Planned Service Areas as outlined in the City’s Comprehensive Plan. These homes are situated on smaller lots and are currently served by both well and septic systems. A lift station that was provided by the development to the southwest, Natures Refuge North, allows these properties to be served by this connection.

We believe the general requirements for a PUD are met in the following ways:

- *Public or common open space to meet minimum requirements established in the ordinance.*
 - The site provides approximately 55 acres of open space, which is just under 73 percent of the site.
- *If PUD includes preservation of natural habitat, those habitats shall be permanently protected through a conservation easement or other legally binding agreement.*
 - We recorded a conservation easement and WMC Buffer Declaration in Natures Refuge and Natures Refuge North and are willing to record similar documents against the open space.
- *Homeowners Association Required.*
 - The lots will be governed by a homeowner’s association (HOA) as further explained below.

SITE PLAN/REZONING

The site plan proposes three street connections: one connecting to the west from Natures Refuge North’s via Haywood Drive, one connecting to the north to Pine Street NE, and one connecting to the east to connect with Nordin Street. Given the tree and wetland impacts as well as existing encroachments into the right-of-way, we are not proposing to connect Nordin Street to Pine Street.

With submittal of the preliminary plat and PUD applications, we will be requesting a rezoning from the R-1 zoning district to the R-2 zoning district with certain flexibilities outlined below. As previously mentioned, this zoning district and requested flexibilities are consistent with what was approved with Natures Refuge North.

Standard	R-2 Zoning Standard	Villa Lots
Minimum Lot Size	7,500 square feet	7,500 square feet
Minimum Lot Width	60 feet	60 feet
Minimum Lot Depth	125 feet	125 feet
Minimum Front Setback	25 feet	25 feet
Minimum Side Setback	10 feet (5 feet for Attached Garage)	7.5 feet**
Minimum Corner Side Setback	25 feet	20 feet
Rear Setback	25 feet	25 feet
Impervious Surface	65%	65%
Collector Road Right-of-Way Width	80 feet	60 feet*

Pine Oaks Concept Plan Submittal

Garage Size	Minimum 20 feet in width and 520 square feet in size	We agree to adhere to these requirements.
Garage Design	Garage shall not protrude forward of the front wall of the house, unless the house includes a front porch that is unenclosed, a minimum of six (6) feet in depth, and extends across 100 percent of the front façade of the house. The garage shall then not protrude forward of the front porch.	We are requesting this be waived but agree that the garage cannot protrude more than five feet forward the house (further explanation in the paragraph below).
Garage Design	The length of the garage wall facing the street shall not be greater than 50 percent of the length of the entire front façade of the structure for street-facing garage doors	Garage walls shall not be greater than 57 percent of the length of the entire front façade (further explanation in the paragraph below – Garage Design Standards).

*This flexibility was granted for Natures Refuge and Natures Refuge North. Haywood Drive from Natures Refuge was granted this flexibility, and the proposed development will extend Haywood Drive to the north. This flexibility is being requested for both Street B and Street A. The street width will be the same width as Haywood Drive.

**We will provide at least 15 feet between homes. The City currently allows a five-foot setback for garages and ten feet for homes, so this is generally consistent with R-2 zoning.

GARAGE DESIGN STANDARDS

In order to minimize impacts to trees, wetlands, and wildlife, we are proposing lot sizes consistent with the R-2 zoning standard with a lot width of 60 feet. R-2 zoning has different garage design requirements than the R-1 zoning district and does not allow the length of the garage to be greater than 50 percent of the length of the entire front façade unless the second floor living space extends over the two-car portion of the garage; the garage doors are decorative and have a strip of windows; garages in excess of two stalls are broken up so the additional stalls are recessed two feet from the main garage stall; and garage width does not exceed 32 feet. Because villas provide single-level living, we cannot provide the living space above the garage on all plans; we only have a few villa plans that are one and a half stories. We also request that we be allowed some flexibility for some of our plans to have garages that protrude no more than five feet forward the house façade. The reason for these requests in flexibility is we cannot get to the 520 square foot garage size with meeting the design standards that are currently in place with single level living homes. In order to meet these requirements, we would need to make the house wider, and we cannot do that with the smaller lots we are proposing in order to conserve as much open space as possible.

In summary, we agree to comply with the garage design standards that were approved for Natures Refuge North:

- Garage shall be allowed to protrude forward of the house, without front porch, a distance of no more than 5', subject to conformance with an anti-monotony plan that prohibits houses with protruding garages to be placed directly adjacent to or across from one another.
- Unless 2nd floor living space extends over the garage, the maximum length of the garage wall facing the street shall not exceed 57% of the front façade subject to the following:
 - Decorative garage doors with a strip of windows are required.
 - Garage width in excess of two stalls will be recessed two feet from the main garage facade.
 - Total garage width will not exceed 32 feet.

TREE PRESERVATION/ECOLOGICAL RESTORATION PLAN/ENVIRONMENTAL ASSESSMENT WORKSHEET

A formal tree preservation plan will be done and submitted with the preliminary plat application, and so at this time, it is not known for sure how many trees will need to be removed on site. We have minimized tree removal as much as possible by reducing lot sizes, opted for slab on grade homes to reduce ponding, and are proposing deeper ponding in concentrated areas rather than spreading out multiple ponds to a larger area. However, in order to avoid as much wetland impact as possible, we anticipate that a significant number of trees will need to be removed in order to accommodate lots and ponding.

In order to avoid wetland, rare plant, and tree impacts, we are preserving a significant amount of open space on the site – approximately 73%. Given that this property is densely forested, it is not feasible to replace the number of trees required to be removed within the development or through paying tree restitution. Therefore, we are proposing to perform ecological restoration on the site in order to offset the effects of tree removal. We have engaged Midwest Natural Resources to develop an ecological plan that will be submitted to the city along with the preliminary plat application. Such plan will include detailed descriptions of the conditions documented at the time of the ecological survey for the avoided areas of the site. The plan will propose management strategies, such as invasive species control, woodland management, potential prescribed burning, or other methods to enhance those communities and maintain habitat for rare plants and resources.

An Environmental Assessment Worksheet (EAW) was done on the property in 2007. It spanned the subject site as well as the properties that are now Natures Refuge and Natures Refuge North and contemplated a unit count of 278. Natures Refuge has 61 units, and Natures Refuge North has an approved 94 units with preliminary plat. With the addition of these 82 proposed units, the total proposed unit count (237 homes) for the area on which an EAW was done is well below the EAW's threshold.

DENSITY

The total site area is 75.42 acres with 31.02 acres of wetlands, 14.37 acres of wetland buffers, and a 0.37-acre gas line easement. The planned density for this area is 1.6-3 units per acre. Netting out wetlands, wetland buffers, and the gas line easement, we are proposing a density of 2.76 units per acre, which is below the allowable maximum density of three units per acre. While we could get to the minimum required density traditional R-1 zoning, the Comprehensive Plan mentions density transfer several times as an incentive for conservation development. First, the Goals and Policies of the Land Use Chapter recognizes that density transfer and bonuses can provide an incentive for achieving specific public values not achievable under conventional zoning standards. The Housing Chapter contemplates density bonuses as an incentive to developers to provide more affordable housing units within conservation subdivisions, as it recognizes that allowing additional units on a site may make a conservation development more financially feasible. The Parks and Trails chapter identifies density transfer as an incentive for setting aside land in a conservation easement or greenway. Snips of this language from the City's Comprehensive Plan are on the following page. In addition to this, the Metropolitan Council has issued its 2050 Land Use Policy, which designates Lino Lakes as a Suburban Edge community, which will require a minimum density of 3.5 units per acre in sewered areas within the City when developed under the guidance of the City's 2050 Comprehensive Plan.

Because a large portion of the site (over 60%) is encumbered by wetlands and their required buffers as well as the gas line easement, and because wetlands, buffers, and utility easements are netted out in determining density numbers, there are many challenges to developing the property with traditional R-1 zoning. Grading costs will be more expensive on this site due to the wetlands and elevations, and the costs of streets and utilities will remain the same regardless of the number of lots on the site. While impacting wetlands would increase the amount of developable acreage on the site in terms of density calculations, we wanted to conserve as many wetlands as possible.

Pine Oaks Concept Plan Submittal

The conservation development plan that was originally proposed for this entire area in the early 2000’s that included this proposed development, Natures Refuge and Natures Refuge North included 278 homes, and we have only proposed a total of 237 homes (41 less homes than was originally contemplated). Additionally, Natures Refuge had a net density of 1.68 units per acre, and Natures Refuge North had a net density of 2.99 units per acre. Looking at the total area of all development in the table below, the average density of the overall site including all three developments is 2.43 units per acre, which is below the maximum 3 units per acre.

Plat Name	Gross Area (Acres)	Wetlands and Buffers (Acres)	Utility Easement (Acres)	Public Parks and Open Space (Acres)	Net Area (Acres)	# of Units	Gross Density (Units/Acre)	Net Density (Units/Acre)
Natures Refuge	94.70	57.06	0.40	0.00	36.26	61	0.64	1.68
Natures Refuge North	59.30	5.31	0.00	22.60	31.39	94	1.59	2.99
Pine Acres	75.42	45.39	0.41	0.00	29.66	82	1.09	2.76
Total/Average	229.42	107.76	0.81	22.60	97.31	237	1.03	2.43

Chapter 3: Land Use 3-3

- f. Identify and define incentives offered to developers for achieving specific public values not achievable under conventional development standards. These incentives may include, but are not limited to:
 1. Density transfer/bonuses
 2. Reduced street and right-of-way widths
 3. Trunk utility and stormwater management fee credits
 4. Staging plan flexibility
 5. Streamlined city approval process
 6. Financial incentives when consistent with city policy

Chapter 4: Housing 4-15

the affordability of these units. Density bonuses may also be used as an incentive to developers to provide more affordable housing units within conservation subdivisions, as allowing for additional units on a site may make a conservation development more financially feasible.

Chapter 10: Parks, Greenways & Trails 10-45

- **Bonus/Incentive Zoning:** Is similar to transferring development rights except that the landowner or developer rights are used by the developer on the same property rather than purchased by another developer for some other property. This could allow a developer to develop at a higher density than normally allowed if the developer sets aside land in a conservation easement or greenway.

ELEVATIONS

Example elevations of our villa product have been provided. These are just an example of the many elevations we offer. We also have anti-monotony requirements to ensure that very similar elevations are not adjacent to one another within the community to preserve visual interest.

RESTRICTIVE COVENANTS

The development will have an HOA that will provide for the maintenance of the overall common elements in the community, including landscaping of the common areas as applicable and management of the natural resources. The HOA will also provide lawn care and snow removal for villa lots. Additionally, restrictions will be placed on outdoor storage and parking (no boats, campers, or trailers) in order to keep the community looking orderly and well maintained.

REQUEST AND CONCLUSIONS

M/I Homes is requesting Board, Commission and City Council feedback and direction on the proposed concept plan for Pine Oaks. We believe that the concept plan proposes a reasonable and high and best use of the property in conformance with the City's Comprehensive Plan.

We respectfully request that the Board, Commission and City Council provide direction and support of the proposed concept plan with the following considerations:

1. The future PUD and neighborhood plan allow for 82 villa lots consistent with the attached concept plan.
2. The proposed density allows the extension of sewer to developed areas of the city currently served by on-site systems through conservation development, which is a goal that is recognized in several chapters of the City's Comprehensive Plan.
3. The minimum lot width requirements conform to the R-2 zoning except as identified in the PUD.
4. In lieu of tree restitution, the City agrees to accept an ecological plan that will restore rare species while controlling invasive species. This could include woodland management, potential prescribed burning, or other methods to enhance those communities and maintain habitat for rare plants and resources.
5. The open space be deeded to the City or to the HOA and protected by deed restrictions or restrictive covenants.

EXAMPLE ELEVATIONS – VILLAS*

*Note that these elevations do not include the garage bumpout proposed in the narrative

The Cedarwood II



The Cedarwood



The Willow II



The Elmwood IV





LEGEND

- RARE PLANT AREA
- EXISTING WETLAND
- EXISTING FLOODPLAIN
- EXISTING GASLINE

NOTES

- 1) No field work has been completed at this time.
- 2) Topography shown is LIDAR which was provided by the Minnesota Department of Natural Resources.
- 3) No Title Work has been furnished for this survey, property is subject to all easements of record, if any.
- 4) Wetlands delineated by Kjolhaug Environmental Services Inc., Nov. 11th, 2025.

SITE DATA

TOTAL SITE AREA	±75.42 AC.	60' SINGLE FAMILY LOTS	
TOTAL ROW AREA	±4.91 AC.	MINIMUM RESIDENTIAL SETBACK DATA:	
TOTAL LOT AREA	±15.65 AC.	FRONT	25 FT.
TOTAL NUMBER OF 60' LOTS	82	SIDE	7.5 FT.
TOTAL OUTLOT AREA	±54.86 AC.	SIDE CORNER	20 FT.
		REAR SETBACK	30 FT.
ROAD LINEAR LENGTH	3,700 L.F.	EXISTING ZONING	R-1
		PROPOSED ZONING	PUD

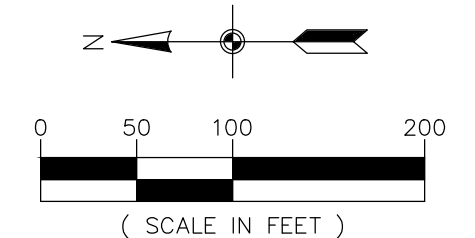
LEGAL DESCRIPTION

(Per Schedule A of Title Commitment File No. ORTE749818, Supplemental No. 2, with a commitment date of November 26, 2024 at 7:00 A.M., prepared by Old Republic National Title Insurance Company)

Lots 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 18, 19, 20, 21, 22 and 23, Block 1; Lots 1, 2, 3, 4, 5, 6, 7A, 23B, 24, 25 and 26, Block 2; Pine Oaks Addition, Anoka County, Minnesota.
(abstract property)

Lots 12, 13, 14, 15, 16, 17, Block 1; Lots 7B, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23A, Block 2; Outlot A, Pine Oaks Addition, Anoka County, Minnesota.
(torrens property)

Being registered land as is evidenced by Certificate of Title No. 113160.



REVIEW COPY

2 of 2

CARLSON ENGINEERING
ENGINEERING \ SURVEYING \ PLANNING

3890 PHEASANT RIDGE DRIVE NE, SUITE 100, BLAINE, MN 55449
TEL 763.489.7900 \ FAX 763.489.7959 \ CARLSON-ENGINEERING.COM

CONCEPT PLAN A

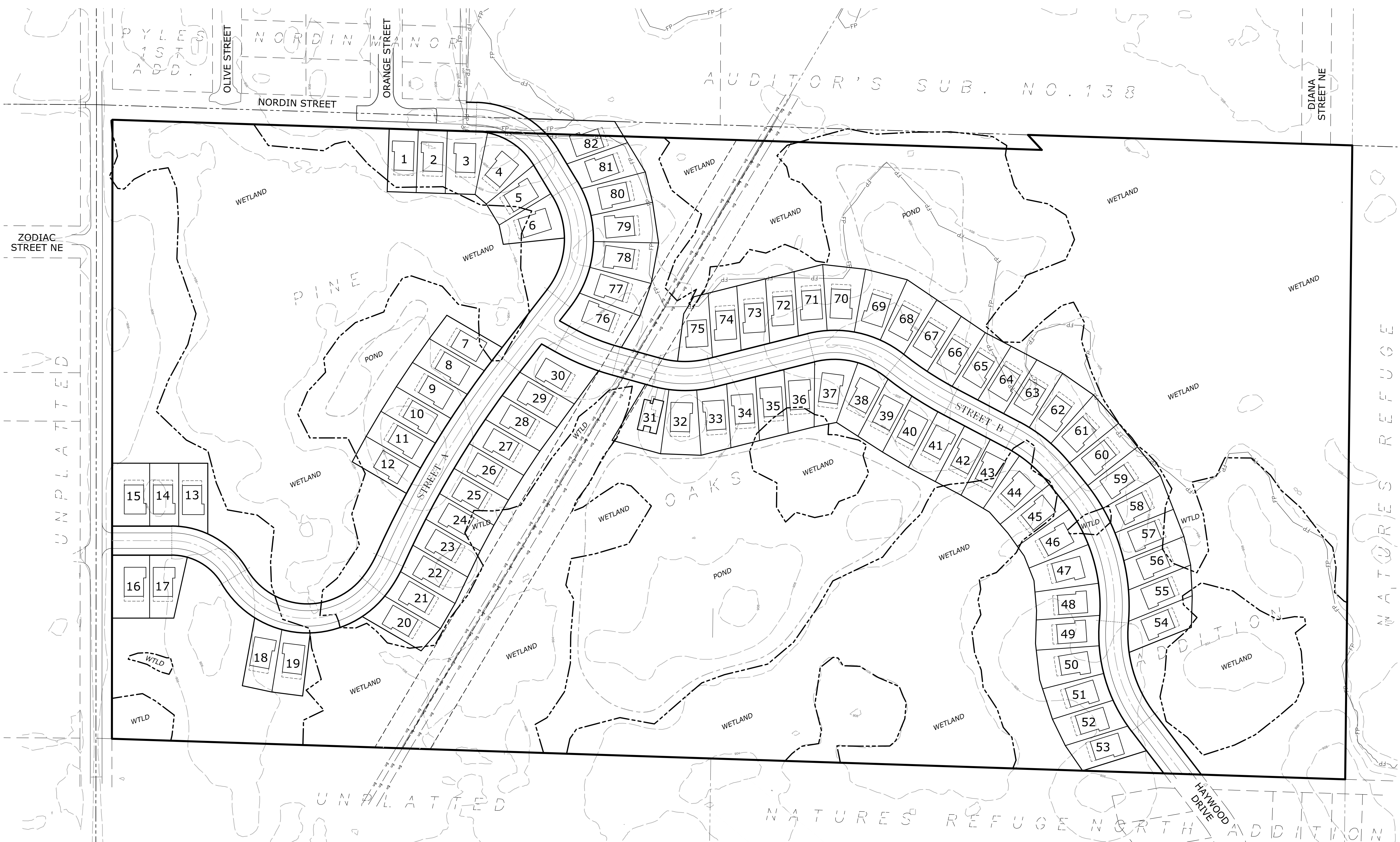
PINE OAKS
Lino Lakes, Minnesota

M/I HOMES OF MINNEAPOLIS, LLC
5354 Parkdale Drive, #100
St. Louis Park, MN 55416

REVISIONS

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DRAWN BY: C#
ISSUE DATE: 11/19/2025
FILE NO: XXX



LEGEND

- RARE PLANT AREA
- EXISTING WETLAND
- EXISTING FLOODPLAIN
- EXISTING GASLINE

NOTES

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- 2) Topography shown is LIDAR which was provided by the Minnesota Department of Natural Resources.
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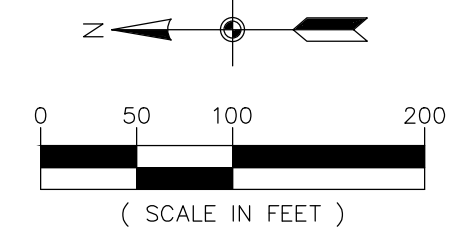
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(abstract property)

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(torrens property)

Being registered land as is evidenced by Certificate of Title No. 113160.



REVISIONS

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ISSUE DATE: 11/19/2025
FILE NO: XXX

1 of 2

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ENGINEERING \ SURVEYING \ PLANNING

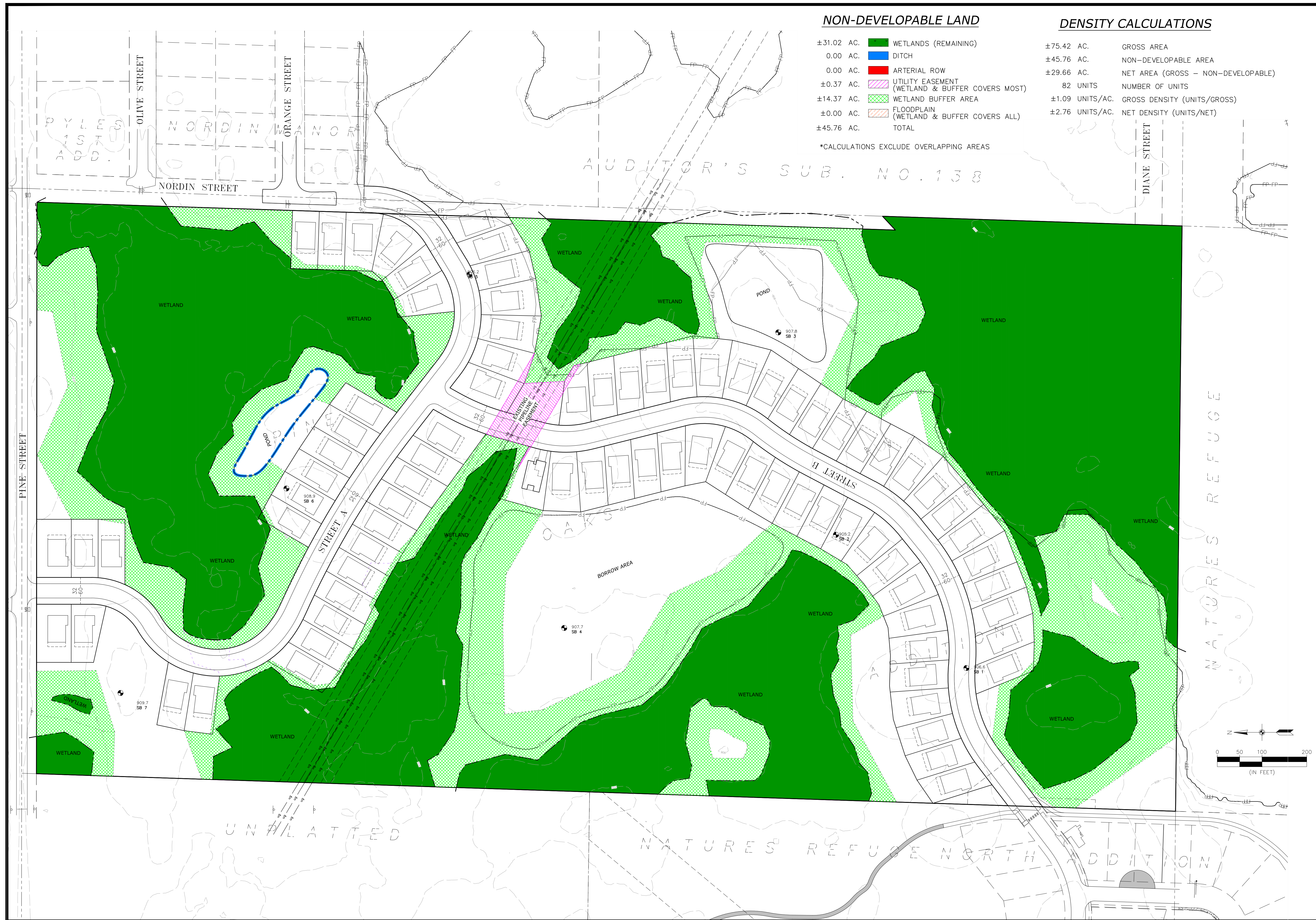
3890 PHEASANT RIDGE DRIVE NE, SUITE 100, BLAINE, MN 55449
TEL 763.489.7900 \ FAX 763.489.7959 \ CARLSON-ENGINEERING.COM

CONCEPT PLAN A

PINE OAKS
Lino Lakes, Minnesota

M/I HOMES OF MINNEAPOLIS, LLC
5354 Parkdale Drive, #100
St. Louis Park, MN 55416

REVIEW COPY



REVISIONS

5.		JJO
6.		JJO
DRAWN BY:		JJO
ISSUE DATE:	XX/XX/XXXX	
FILE NO:	XXX	

REVIEW COPY

Site: DBE: 11/21/23 | User: 5400 - 5400/5412-04 - nature's_refuge_north | 5412-04_net_area_pfm.dwg

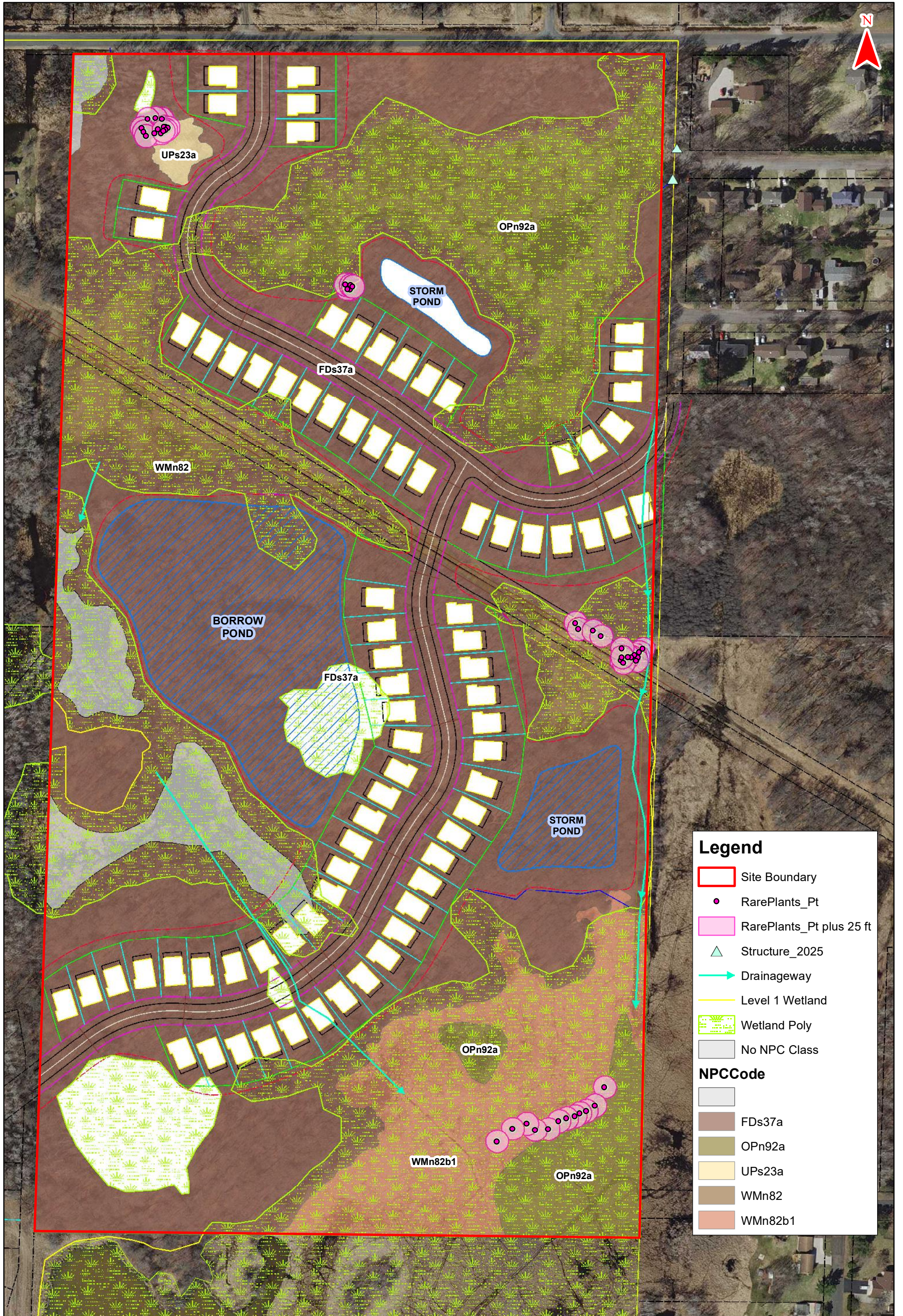


Figure 3 - Site Plan with Mapped Native Plant Communities & Rare Species



Pine Oaks (KES 2025-101)
Lino Lakes, Minnesota



Memorandum

To: Katie Larsen, Lino Lakes City Planner

From: Kris Keller PE, WSB
Diane Hankee PE, Lino Lakes City Engineer

Date: January 7, 2026

Re: Pine Oaks (MI Homes)
Concept Plan Review
032826-000

WSB's review of the Concept Plans for the Pine Oaks (MI Homes) development in Lino Lakes, MN, prepared by Carlson Engineering and received December 12, 2025. Our comments were made on the following documents:

- Pine Oaks Concept Plan prepared by Carlson Engineering, dated November 19, 2025.

The following review comments should be responded to in writing by the applicant.

Engineering

- **General**

The Pine Oaks development is a replatting of the same named development that was originally platted in 1980. It includes 75 acres north of CSAH 14 (Main Street), west of CSAH 23 (Lake Street), and south of Pine Street adjacent to Natures Refuge North. The current conceptual project proposes 82 single family villa lots and associated infrastructure. This property was originally part of a larger development of which an Environmental Assessment Worksheet (EAW) was prepared. The evaluation concluded that the project did not have the potential for significant environmental effects.

- **Grading**

The Pine Oaks development includes site grading for the 82 units as well as extensions of Haywood Drive and Nordin Street as well as a local street that is yet to be named.

- **Stormwater Management**

Stormwater management for the Pine Oaks development has not yet been proposed and will need to be submitted for preliminary plat by the applicant. It is anticipated that the development will include a storm sewer conveyance, rear yard drainage, and ponding systems, consistent with the previous and adjacent developments. The systems shall meet the City's and Rice Creek Watershed District's (RCWD) Stormwater Management requirements. Stormwater reuse will be required for irrigation. Access to the ponds for maintenance per City requirements will also need to be provided with plans.

- **Water Supply**

The Pine Oaks project is to be served by municipal water supply. It is anticipated that the existing 16" DIP trunk main on Haywood Drive will need to be extended northeast through the development to Orange Street. The City's 2040 Comprehensive Plan requires this trunk line to be extended to the east. Applicant to verify water pressure is adequate for all locations within the development.

Applicant is to provide water demand calculations so the City can evaluate available water supply. The City's water supply system well firm capacity is currently constrained by poor water quality in some wells, and annual variability in water use due to drought and other factors. Based on the water supply capacity and well trigger analysis completed in April 2024, the city may need additional well(s) to accommodate development within the City's existing 2040 Utility Staging Area. While the City's Comprehensive Plan identifies the need for additional wells to support continued growth, the White Bear Lake District Court orders and ongoing litigation, currently prevents the City from implementing that plan.

- **Sanitary Sewer**

The Pine Oaks project is to be served by municipal sanitary sewer system. It is anticipated that the existing 8- inch diameter poly-vinyl chloride (PVC) trunk main on Haywood Drive will be extended north to serve the proposed addition. The development will be served by the lift station, recently installed with the Natures Refuge North development. The trunk sanitary sewer main will be extended to the northeast, consistent with the City's 2040 Comprehensive Plan.

- **Transportation**

A general street layout has been proposed with the Concept Plan that differs from the original 1980 plat which is now coordinated with existing adjacent developments. The City of Lino Lakes' Comprehensive Plan requires neighborhood street connections. The applicant is showing a connection with rights of way to Nordin Street to the east and Pine Street to the north. A traffic study will be required and reviewed with the preliminary plat.

Trails and sidewalks

No trails nor sidewalks are shown along any of the streets on the submitted Concept Plan. Additional trail and sidewalk segments may need to be included throughout the site at the direction of the City Planner.

- **Landscaping**

A Landscape Plan for Pine Oaks Addition has not yet been submitted. One will need to be submitted and reviewed by staff and the City's Environmental Coordinator. The Environmental Coordinator will provide separate comments.

- **Floodplain**

The Pine Oaks Addition potentially impacts the existing floodplain and a letter of map revision will be required.

- **Drainage and Utility Easements**

Roadways are proposed within right-of-way and lots include standard drainage and utility easements. Larger drainage and utility easements will be required to be provided over any utilities outside the right of way and any surface water management features

- **Development Agreement**

A development agreement will be required with the final plat.

- **Grading Agreement**

Grading agreement not required at this time.

- **Stormwater Maintenance Agreement**

The stormwater maintenance for public facilities in the Pine Oaks development will be covered under the City's Programmatic Stormwater Management Agreement after the facilities have been installed and accepted by the City. Borrow areas / Lakes area will be maintained by the HOA.

- **Permits Required**

Required permits will be determined at the time of preliminary plat submittal. Potential permits may include some of the following and others:

1. NPDES General Construction Permit
2. City of Lino Lakes Zoning Permit for Construction
3. Minnesota Pollution Control Agency Sanitary Sewer Extension Permit
4. Minnesota Department of Health
5. Rice Creek Watershed District Permit
6. US Army Corps of Engineers Section 404 Permit

If you or the applicant have any questions regarding these comments, please contact Kris Keller at (612) 419-3083 or kkeller@wsbeng.com. You may also contact Diane Hanke at (651) 982-2430 or dhanke@linolakes.us.



Environmental Memo

To: Katie Larsen, City Planner

From: Tom Hoffman, Environmental Coordinator

Date: December 17, 2025

Re: Environmental Comments – Pine Oaks Concept Plan

Environmental Board had recommended the following at their December 17, 2025 meeting:

1. Consideration should be given to the proposal to retain permanent open space in lieu of tree mitigation subject to the 3 trees per lot requirement.
 - a. Outlots adjacent to Wollans park dedicated to the City.
 - b. Conservation easements over the remaining open space.
2. Open area landscape requirements should be adhered to in areas around stormwater and borrow ponds.
3. A new rare plant and animal survey must be completed for the project area.
 - a. Updated NHIS letter from the DNR, 2007 listed Blandings Turtle. Additional species of concern may be present since the last update.
 - b. Overlay threatened and endangered species map with concept plan to show proposed impacts.
4. If rare species are found on site, design layout should be revised to avoid impacts to these species.
5. Project narrative states 31.02 acres of wetland, delineation report has 34.55 acres of wetland shown. Staff will use the delineation report of 34.55 acres of wetland. This should be reflected in all places within the next proposal for accuracy.
6. If rare species are found on site and impacts are unavoidable, include features that minimize impacts (ex: buffers around rare plant locations for any that can be preserved, surmountable curbs to facilitate Blanding's turtle movement, restoration of undisturbed project areas). DNR permits may be required for any impacts to rare species.
7. Coordinate with the Rare Plant Rescue group (contact Carrie Taylor at Anoka Conservation District) to allow for transport of any rare plants for which impact is unavoidable to a donor site, such as Wollan's Park Wetland Bank in Lino Lakes.

8. Provide an updated tree inventory showing trees within the parcel. Provide a summary of trees based on ID.
9. Tree protection zones in accordance with the City Tree Protection Fence Detail must be shown for all trees that are being preserved. These zones must be shown on the Tree Preservation Plan and Grading Plans submitted at the Preliminary Plat phase.
10. Tree removal should be completed outside of the growing season. This will limit impacts to nesting species and prevent the spread of oak wilt.
 - a. Dormant trimming and removal of oaks is highly recommended in this area, not only to mitigate for potential spread of the fungus *Bretziella fagacearum*. Also to reduce impacts on cavity nesting species which are extremely common in this area.
11. Consider extension of trails into the upland areas.
12. Consider incorporation of a stormwater reuse system for lawn irrigation.
13. Site is located within the Rice Creek Watershed District Wetland Management Corridor, wetlands and buffers will be required to meet the requirements for the WMC.
14. MNR to provide a management plan for the protected open space. This plan should include recommendations for management and maintenance. This includes the contiguous area from Natures Refuge North as part of this plan.
 - a. Management of the entire open space/greenway system through the adjacent developments should be incorporated.
15. Create an out lot following Wetland 7 & 8 to be deeded to the City for ownership and future restoration of the wetland.
16. Shift Haywood Drive North to preserve additional buffers to the south around Wetlands 7 & 8.
17. Limit stormwater and borrow pits to one area if feasible. Creating additional impacts in one area is preferred to creating impacts in multiple locations, specifically in the upland between lots 67-70. If this area can be decreased by moving it adjacent to the wetland to preserve trees or combine it with proposed impacts to the large basin to the west.

**PLANNING & ZONING BOARD
AGENDA ITEM 7A**

STAFF ORIGINATOR: Michael Grochala, Community Development Director

MEETING DATE: January 14, 2025

TOPIC: Metropolitan Council 2050 System Statement

CASE NUMBER: N/A

APPLICANT: City of Lino Lakes

OWNER: N/A

REVIEW SCHEDULE: N/A

BACKGROUND

Minn. Stat. 473.145 requires the Metropolitan Council (Met Council) to adopt a comprehensive development guide for the metropolitan area. Minn. Stat. 473.175 requires the Met Council to review local government comprehensive plans to determine compatibility with this regional development guide. These plans are developed and updated every 10 years.

In early 2025, the Met Council adopted Imagine 2050 as the comprehensive regional development guide. It establishes the regional vision for the 7-county metropolitan area for the next 25 years. The guide includes 5 policy plans:

1. 2050 Land Use Plan
2. 2050 Transportation Policy Plan
3. 2050 Water Resources Policy Plan
4. 2050 Regional Parks Policy Plan
5. 2050 Housing Plan

On September 25, 2025, the Met Council issued the 2025 System Statement for Lino Lakes. The system statement explains the implications of the policy plans for each individual community. Pursuant to Minn. State 473.864, subd. 2, within 3 years after receiving a system statement Lino Lakes is required to review its comprehensive plan to ensure continued conformity with metropolitan system plans and amend if necessary. The amendment/update of the City's 2040 Comprehensive Plan is due by December 31, 2028.

ANALYSIS

Community Designation

Imagine 2050 identifies Lino Lakes with the community designation of Suburban Edge. The City's prior designation of Emerging Suburban Edge has been eliminated. Suburban Edge communities include cities, townships and portions of both that are primarily located on the edge of the Metropolitan Urban Service Area (MUSA) and primarily developed after the 1990's. Other Anoka County and surrounding communities that share this designation include Centerville, Blaine, Andover, Ramsey, Hugo, and Forest Lake.

Residential density requirements

Communities designated Suburban Edge are expected to plan for forecasted population and household growth at a minimum average net density of 3.5 - units per acre for new development and redevelopment.

From Imagine 2050 Regional Development Guide: Local governments and land within the MUSA receive a higher level of regional services and investments than those in the rural service area, such as regional wastewater services, regional highways, transit service, and programs that support redevelopment. In return, the Met Council expects jurisdictions in the MUSA to plan for and build the higher levels of development that economically support those regional services. Compact development also reduces the pressure on agricultural land areas and decreases the impact of development on natural systems which helps manage the region's land supply consistently and more effectively.

As discussed with the City Council in the fall of 2024, the minimum average density was originally proposed to increase from the existing 3.0 units per acre to 4.0. The 3.5 units per acres was ultimately what was adopted by Metropolitan Council.

Forecasts

Imagine 2050 forecasts for Lino Lakes' population, households and employment are:

	2020 (actual)	2030	2040	2050	Change
Population	21,399	24,600	26,600	29,200	4,600
Households	6,957	8,300	9,200	10,200	1,900
Employment	3,786	5,200	5,600	6,000	800

Overall, the 2050 population and household forecasts show a decrease in what is currently for in the 2040 plan. The following chart compares the City’s current 2040 Comprehensive Plan projections with Imagine 2050.

	Comp Plan 2040	Thrive 2050	Difference
Population	31,100	29,200	(1,900)
Households	10,600	10,200	(400)
Employment	6,000	6,000	NC

The forecasted 2050 population projections decrease by 1,900 persons and 400 households over what is currently forecasted for 2040. The reduction in forecasted growth is, in part, due to the continued evaluation and study of the northeast metro water supply constraints.

Employment projections are expected to remain the same.

Housing Policy Plan

Communities are required to include a housing element and implementation program in their comprehensive plan. Minn. Stat. 473.859, Subd 2(c) states:

“A land use plan must also include a housing element containing standards, plans and programs for providing adequate housing opportunities to meet existing and projected local and regional housing needs, including but not limited to the use of official controls and land use planning to promote the availability of land for the development of low- and moderate-income housing.”

For 2025, the rental affordability limit is 60% and the ownership affordability limit is 80% of the area median income (AMI). In 2025, the area median income for a household of four is \$132,400. The current affordability amounts are as follows:

2025 Rental housing

# Bedrooms	30% AMI	50% AMI	60% AMI	80% AMI
Efficiency	\$694	\$1,158	\$1,389	\$1,852
1 Bedroom	\$744	\$1,241	\$1,489	\$1,985
2 Bedrooms	\$894	\$1,490	\$1,788	\$2,384
3 Bedrooms	\$1,032	\$1,721	\$2,065	\$2,753
4 Bedroom	\$1,152	\$1,920	\$2,304	\$3,072

Ownership housing

For owner-occupied housing, the income limit includes principal, interest, property taxes and home insurance.

Family Income Level	Affordable Home Price
80% AMI (\$104,200)	\$306,500
60% AMI (\$79,440)	\$225,300
50% AMI (\$66,200)	\$183,700
30% AMI (\$39,700)	\$98,000

The Region’s Total Future Need for Affordable Housing for 2031 to 2040 is forecasted to be 39,700 units. Per the System Statement, Lino Lakes’ share of the region’s need for low- and moderate-income housing for the decade is 428 new units affordable to households up to 60% of area median income (AMI) or below. In comparison, the Met Council’s goal for 2021-2030 was 515 units up to 80% of area median income.

Affordable Housing Unit Needs for Lino Lakes	
Threshold	# Housing Units
At or below 30% AMI	207
31 to 50% AMI	190
51 to 80% AMI	31
Total Units	428

To accommodate our share of the region’s affordable housing need, we are required to guide residential land at densities sufficient to create opportunities for affordable housing. The City may choose between two options:

Option 1: Guide sufficient land at minimum residential densities of 10 units/acre to support your city or township's total allocation of future affordable housing need for 2031-2040

Or

Option 2: Guide sufficient land at minimum residential densities of:

- 12 units/acre to support your city or township's 2031-2040 allocation of future affordable housing need at 30% AMI or less.
- 8 units/acre to support your city or township's allocation of future affordable housing need at 31-60% AMI. This combines your city or township's allocation of 31-50% AMI and 51-60% AMI.

The City chose to implement option 1 in the 2040 plan. However, staff notes that the densities in Imagine 2050 represents a change from a minimum of 8 units/acre to 10 units/acre.

Under Option 1 we can guide 42.8 acres of residential land at 10 units per acre to satisfy our affordable housing unit needs (428 units/10 units per acre = 42.8 acres). Alternatively, the City could choose to guide 17.25 acres of residential land at 12 units per acre and 27.6 acres at 8 units per acre under Option 2 to meet the minimum requirements.

Currently the City's Planned Residential/Commercial, and Signature Gateway Land Use Districts allow 8-10 units per acre. The Town Center District allows 10 – 24 units per acre. The City does not currently have a district that has a minimum density of 12 units per acre.

This requirement will require some modification to our individual land use district density requirements.

Climate Policy

New requirements were added by the State Legislature in 2023 to address climate mitigation and adaptation. This will include a plan that acknowledges statewide targets for GHG reductions of 50% by 2030 and net zero by 2050, greenhouse gas emission inventory and at least one strategy to reduce greenhouse gas emissions.

Transportation Policy Plan

The 2050 Transportation Policy Plan is the metropolitan system plan for highways, transit and aviation to which local comprehensive plans must conform. The only major improvement to the highway (I-35E, I-35W or CR 14), transit (Metro Transit) or aviation (Seaplane Base) systems, identified in the System Statement, is the reconstruction of the I-35E/CRJ interchange currently underway. During the comprehensive plan update, we will identify potential transit service options and review any updates to Met Council's aviation long term comprehensive plan.

Other plan considerations include Regional Bicycle Transportation Network, Minor System/Functional Classification and Freight changes. The Regional Bicycle Transportation Network (RBTN) identifies trunk arterials of the overall bikeways system that connect to regional employment and activity centers. The RBTN identifies routes along CR 14 (Main Street), CSAH 23 (Lake Drive) and CSAH 21 (Centerville Road) and through the Regional Park and these routes have already been incorporated into the City's Park, Greenway and Trail System Plan. Lino Lakes will describe and map the functional classification of all existing roads and identify any changes from the existing to proposed classifications. No freight terminals are identified in Lino Lakes.

Water Resources Policy Plan

The Water Resources Plan takes an integrated approach to water supply, water quality and wastewater issues and identifies key concepts to maximize the benefits of regional investments into wastewater, water supply and surface water. Lino Lakes shall develop components in the

comprehensive plan to address wastewater, surface water and water supply issues. The City currently will be updating its Surface Water Management Plan and Water Supply Plans in 2026-27.

Regional Parks Policy Plan

The Met Council Regional Parks System includes 66 regional parks and 487 miles of regional trails. Regional parks and trails within Lino Lakes are:

- Park Reserves
 - Rice Creek Chain of Lakes Park Reserve
- Regional Trails
 - Bunker Hills-Chain of Lakes Regional Trail
 - Central Anoka Trail
 - Chain of Lakes – Otter Lake Regional Trail
 - Rice Creek North Regional Trail

Anoka County is one of Met Council’s ten regional park implementing agencies and Lino Lakes will continue to work with the County on regional park development. The above parks and trails are currently addressed in the City’s 2040 Comprehensive Plan.

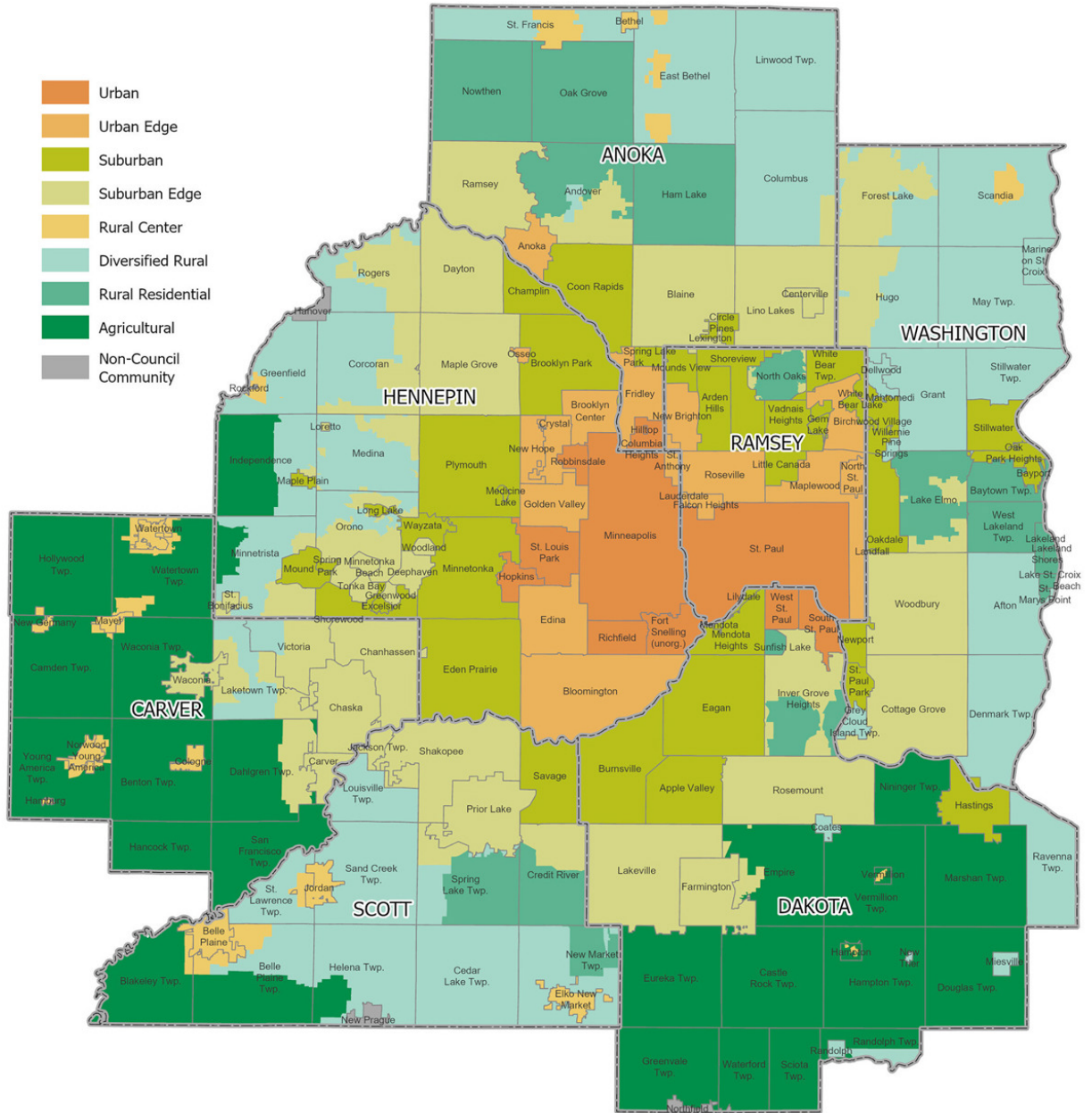
RECOMMENDATION

Discussion Only.

ATTACHMENTS

1. Community Designation Map
2. 2050 System Statement

Imagine 2050 Community Designations





**CITY OF
LINO LAKES**

2025 SYSTEM STATEMENT



IMAGINE²⁰₅₀



2025 SYSTEMS STATEMENT CITY OF LINO LAKES

Regional Development Plan Adoption

On February 12, 2025, the Met Council adopted Imagine 2050 as a comprehensive development guide for the Twin Cities metro region. Imagine 2050 includes regional systems plans, including the 2050 Transportation Policy Plan, the 2050 Water Policy Plan, the 2050 Regional Parks and Trails Policy Plan, and the 2050 Housing Policy Plan. The development guide also outlines land use policies and community designations important for local comprehensive planning updates.

The Metropolitan Council is now issuing systems statements pursuant to [State statute](#).

Systems Statements

Metropolitan systems plans are long-range comprehensive plans for the regional systems – transit, highways, and airports; wastewater services; and parks and open space – along with the capital budgets for those systems. Systems statements explain the implications of metropolitan system plans for each community in the region. They are intended to help communities prepare or update their comprehensive plan, as required by the Metropolitan Land Planning Act:

Within nine months after receiving a system statement for an amendment to a metropolitan system plan, and within three years after receiving a system statement issued in conjunction with the decennial review required under [section 473.864, subdivision 2](#), each affected local governmental unit shall review its comprehensive plan to determine if an amendment is necessary to ensure continued conformity with metropolitan system plans. If an amendment is necessary, the governmental unit shall prepare the amendment and submit it to the council for review.

The system statement includes information specific to a community, including:

- community designation or designation(s)
- forecasted population, households, and employment through the year 2050
- guidance on appropriate densities to ensure that regional services and costly regional infrastructure can be provided as efficiently as possible
- affordable housing need allocation.

In the following sections, this systems statement contains an overview of key changes and updates to the policies and plans from the previous 2040 regional development guide and specific system changes that affect your community:

- Transportation, including metropolitan highways, aviation, and transit
- Water Resources, including wastewater, surface water, and water supply planning

- Regional parks and trails

Regional Development Guide

The development guide adopted in February 2025 reflects the vision and plans adopted by communities for a prosperous, equitable, and resilient region with abundant opportunities for all to live, work, play, and thrive.

Regional values are shared core beliefs or principles that guide the work of developing and implementing Imagine 2050. The values build on those identified in Thrive MSP 2040 and incorporate what we've learned over the last ten years as well as the common values expressed by local governments and partners across the region. These values guide the Met Council in building partnerships and developing policies and programs to support Imagine 2050.

The Met Council has endorsed the following goals for our region to achieve through our policies, practices, programs, and partnerships:

- Our region is equitable and inclusive.
- Our communities are healthy and safe.
- Our region is dynamic and resilient.
- We lead on addressing climate change.
- We protect and restore natural systems.

Dispute Process

If your community disagrees with elements of this systems statement or has any questions about the process, please contact your Sector Representative, Emma Dvorak, at 651-602-1399, to review and discuss potential issues or concerns. The Council and local government units and districts have usually resolved issues relating to the system statement through discussion.

Request for Hearing

If a local governmental unit and the Met Council are unable to resolve disagreements over the content of a system statement, the unit or district may, by resolution, request that a hearing be conducted by the Met Council's Land Use Advisory Committee or by the State Office of Administrative Hearings to consider amendments to the system statement. According to Minnesota Statutes [section 473.857](#), the request shall be made by the local governmental unit or school district within 60 days after receiving the system statement. If the Met Council does not receive a hearing request within 60 days, the statement becomes final.

Next Steps in the Regional Planning Cycle

Receipt of this system statement and the metropolitan system plans triggers a community's obligation to review and amend its comprehensive plan by December 2028. Local comprehensive plans and amendments will be reviewed by the Met Council for conformance to

metropolitan system plans, consistency with Met Council policies, and compatibility with adjacent and affected governmental units. Updated local comprehensive plans are due to the Met Council for review by Dec. 31, 2028.

Forecasts

The Met Council uses the forecasts developed as part of Imagine 2050 to plan for regional systems. Communities should base their planning work on these forecasts. Given the nature of long-range forecasts and the planning timeline undertaken by most communities, the Met Council will maintain on-going dialogue with communities to consider any changes in growth that may have an impact on regional systems.

Imagine Forecasts for the City of Lino Lakes

	2020 (actual)	2030	2040	2050
Population	21,399	24,600	26,600	29,200
Households	6,957	8,300	9,200	10,200
Employment	3,786	5,200	5,600	6,000

Housing Policy

The purpose of the Housing Policy Plan, adopted by the Metropolitan Council in February 2025, is to provide leadership and guidance on regional housing needs and challenges and to support Imagine 2050. The Housing Policy Plan provides an integrated policy framework that unifies our existing roles in housing and identifies opportunities to expand our role in supporting safe, affordable and dignified housing in the region.

Consistent with state statute (Minn. Stat. 473.859, subd. 2(c) and subd. 4), cities and townships must include a housing element and implementation program in their local comprehensive plans that address existing and projected housing needs.

The Met Council has determined the regional need for low-income housing for the decade of 2031-2040 (see Appendix B in the Housing Policy Plan).

Based on regional forecasts, the regional need for future affordable housing units is 39,700. Lino Lake' share of the region's future need for low-income housing is 428 new units of affordable housing. Of these new units, the need is for 207 affordable to households earning at or below 30% of AMI, 190 affordable to households earning 31% to 50% of AMI, and 31 affordable to households earning 51% to 60% of AMI.

Affordable Housing Need Allocation for the City of Lino Lakes

	Units
At or below 30% AMI	207
31 to 50% AMI	190
51 to 60% AMI	31
Total Units	428

Future Affordable Housing Need allocations may be updated throughout the decade if local projected sewerage growth changes.

Lino Lakes should consult the complete Housing Policy Plan when preparing its local comprehensive plan. In addition, Lino Lakes should consult Imagine 2050 and the Local Planning Handbook for specific requirements necessary for the housing element and housing implementation programs of local comprehensive plans.

Climate Policy

In 2023, the State legislature amended the Metropolitan Land Planning Act ([Minn Stat. § 473.859, subd. 2 and 7](#)) to include new requirements that comprehensive plans address climate mitigation and adaptation. In climate policy, mitigation strategies focus on minimizing contributions to climate change through efficiency measures and reducing greenhouse gas emissions. Adaptation strategies focus on how to change policies and practices to adjust to ongoing and future impacts of climate change.

The Met Council will assist communities in addressing climate mitigation and adaptation elements by providing communities with greenhouse gas inventories and technical assistance for identifying appropriate strategies.

Community Designation

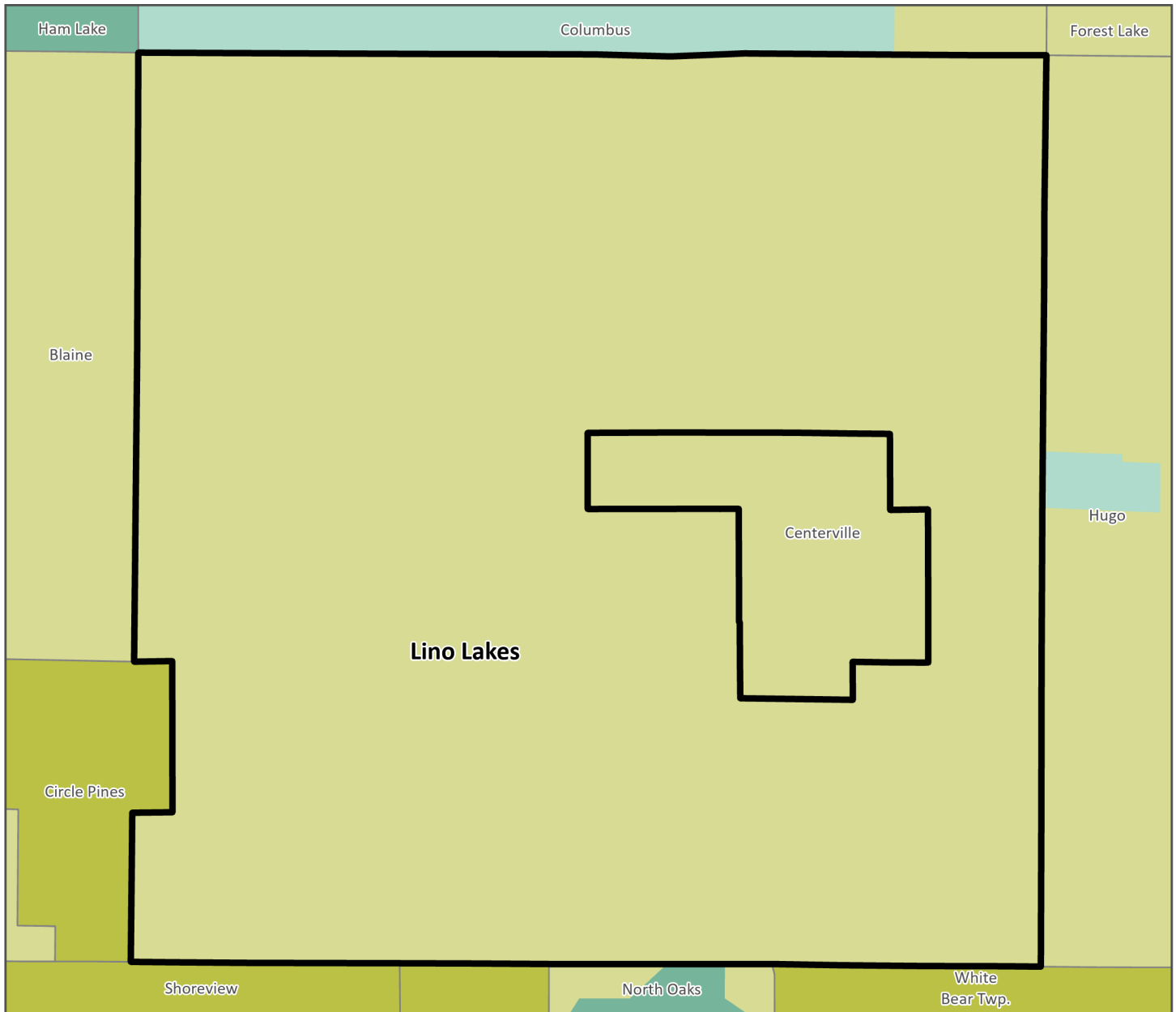
Community designations group jurisdictions with similar characteristics for the application of regional policies. The Council uses community designations to guide regional growth and development; establish land use expectations including overall development densities and patterns; and outline the respective roles of the Council and individual communities, along with strategies for planning for forecasted growth. If there are discrepancies between the Imagine 2050 Community Designations Map and the Community Designation map contained within this systems statement, communities should follow the specific guidance contained in this document. Imagine identifies the City of Lino Lakes with the community designation of Suburban Edge on the Community Designation map.

Suburban Edge municipalities are on the edge of the Metropolitan Urban Service Area (MUSA) and primarily developed after the 1990s. While denser downtown or core areas are often present, the predominate development pattern is low or medium density residential subdivisions characterized by cul-de-sacs and limited access to major thoroughfares for traffic movement. These areas also feature extensive undeveloped land planned for low to medium residential densities along with some areas guided for industrial and commercial uses, often near highway intersections. The planning challenge in Suburban Edge areas is to improve street connectivity and integrate sustainable development practices, ensuring that growth and conservation effectively coexist. Suburban Edge cities may have more than one community designation based on forecasted growth and current or planned development patterns. This happens generally when areas in a municipality either have an historic development pattern more appropriate for a different designation or portions of the municipality are not currently planned for future development and are still considered rural or agricultural in nature.

Specific strategies for Lino Lakes can be found on your [Community Page](#) in the [Local Planning Handbook](#).

City of Lino Lakes Community Designations

Suburban Edge



Urban Service Areas

- Urban
- Urban Edge
- Suburban
- Suburban Edge

Rural Service Areas

- Rural Center
- Diversified Rural
- Rural Residential
- Agricultural
- Non-Council Community

2025 TRANSPORTATION SYSTEM STATEMENT CITY OF LINO LAKES

Transportation Policy Plan Overview

The 2050 *Transportation Policy Plan* (TPP) is the metropolitan system plan for transportation, including highways, transit, bicycle, pedestrian, freight, aviation, and travel demand management, to which local comprehensive plans must conform. The TPP also includes regional transportation objectives, supporting policies and actions, and regional performance measures. The full TPP can be found at the following url:

<https://imagine2050.metrocouncil.org/chapters/transportation>.

This system statement summarizes the regional systems and any significant changes to them, other important regional transportation considerations, and changes made to the 2050 TPP since the 2040 TPP was adopted in 2015. Additionally, this system statement highlights those elements of the system plan that apply to your community. The TPP incorporates policy direction and new 2050 socioeconomic forecasts adopted by the Metropolitan Council in Imagine 2050 and extends the planning horizon from 2040 to 2050.

Federal and State Requirements

The TPP is a document that meets both state requirements outlined in statute for a metropolitan system plan, and federal requirements as a fiscally constrained long-range investment plan for surface transportation. As the region has recently received a significant number of new and increased sources of transportation revenue, the 2050 TPP no longer includes an increased revenue scenario, but it does include additional opportunities for investment. The plan also continues to assume competitive transportation funding (e.g., state and federal discretionary programs like INFRA and Corridors of Commerce) will be sought and amended into the plan as awarded.

Under the Metropolitan Land Planning Act, local comprehensive plans are expected to conform to the projects currently identified as funded in the fiscally constrained plan, which is the official metropolitan system plan. The additional opportunities for investment may be identified separately in local plans as unfunded proposals but are not required to be included.

Federal requirements also call for metropolitan transportation plans to be performance based, so the TPP includes objectives, policies, and actions for the regional transportation system. These objectives, policies, and actions guide the Council and regional partners in their planning and investment in the surface transportation system.

In addition to reviewing this system statement, consult the TPP to ensure that your community's local comprehensive plan and plan amendments conform to the metropolitan transportation system plan. Communities should also review the Imagine 2050 Land Use Policy for land use considerations near certain transportation infrastructure. The format of this plan is different than past TPPs. The plan is no longer separated by chapter, rather each modal plan is distinguished from each other as individual investment or system plans. If the Council has a role in the funding of a regional system, the plan is identified as an investment

plan, if not, then it is identified as a system plan. There is no appendix in the 2050 TPP, supporting documentation is combined directly with the specific investment or system plan and identified as support documentation.

2050 Transportation Policy Plan Regional Systems

Adopted by the Metropolitan Council in February 2025, the TPP identifies the regional transportation systems, regional transportation objectives and policies, and other regional transportation issues communities should consider when producing their own comprehensive plans. This section will cover descriptions of each system and key changes made to these systems in the TPP.

Regional Transportation Policies and Actions

In addition to identifying regional systems and investment needs, the TPP includes a set of policies and related actions to guide investments, planning and priorities for the regional transportation system. The policies and actions cover many topics including safety, highway planning priorities, complete streets, non-motorized transportation, equity, climate and natural systems. Communities are encouraged to incorporate regional policies in their local plans.

Regional Highway System

The Metropolitan Highway System is made up of principal arterials, shown in Figure 1 of the *Highway Investment Plan* and included with this system statement. This system is federally known as the National Highway System. The Metropolitan Highway System is a vital element of the regional transportation system; while minor arterials are lower order roadways that provide connectivity to and relieve congestion in the principal arterial system. Principal and minor arterials combined represent the Regional Highway System.

Key Changes from the 2040 TPP

- A major shift from previous TPPs is that potential new revenues are no longer considered a means to resolve unmet needs on the system. The 2050 TPP does not have a current and increased revenue scenario as a result. Instead, the 2050 TPP identifies funded projects in the fiscally constrained plan and additional opportunities beyond what is funded. Multiple studies completed since the 2040 TPP identified additional opportunities for investment, these are shown in figures 8 and 10.
- The 2050 TPP identifies highway investments in five major categories:
 - Safety: Figure 7 in the *Highway Investment Plan* shows regionally significant safety investments.
 - Spot mobility: Figure 8 in the *Highway Investment Plan* shows funded regionally significant spot mobility projects alongside identified additional opportunities for investment.
 - Interchanges: Figure 9 in the *Highway Investment Plan* shows funded and planned regionally significant interchange investments.
 - Managed lanes: Figure 10 in the *Highway Investment Plan* shows the existing, under construction and planned E-ZPass system. It also includes managed lane corridors which are identified additional opportunities for investment.

- Targeted regional capacity: Figure 11 in the *Highway Investment Plan* shows funded and planned regionally significant highway capacity investments. It also includes potential future capacity enhancements identified in studies.
- Modifications were made to the 2050 TPP that removed appendix naming found in previous plans and reworks previous chapters. Support documents for the *Highway Investment Plan* are now: Functional Classification, Preliminary Interchange Approval Process, and Congestion Management Process. The project list appendix item is now a separate section of the TPP, titled Long Range Highway and Transit Capital Project List. This updated list identifies projects from 2025 to 2050.

Regional Transit System

The regional transit system is comprised of the region’s transitways, regular route bus services, demand response services, transit centers and park and rides, and transit advantages. This system is operated by six different service providers in the region. The COVID-19 pandemic upended transit, and some services vary dramatically from pre-pandemic levels. Many routes were suspended in the region and are still in the process of restructuring.

Key Changes from the 2040 TPP

- A major shift from previous TPPs is from new revenues raised to fill budget gaps in the system. A new source of revenue for transit was created in 2023 to fund operations and maintenance for the region. Similar to highways, the *Transit Investment Plan* no longer contains a current and increased revenue scenario. The projects shown in the 2050 TPP are those in the fiscally constrained plan. Additional opportunities for investment are also identified in the plan and these may be incorporated into local comprehensive plans.
- Several transitways have been implemented and opened and additional transitways have been identified in the region. Multiple arterial bus rapid transit (BRT) routes have been completed and put into operation, while more alignments have been identified in studies for future implementation. The *Transit Investment Plan* identifies investments planned to be implemented in the 2050 TPP:
 - Two light rail extensions (Green Line Extension, Blue Line Extension)
 - Two dedicated BRT routes (Gold Line, Purple Line)
 - Five arterial BRT routes (B Line, E Line, F Line, G Line, H Line)
- Modifications were made to the 2050 TPP that removed appendix naming found in previous plans and reworks previous chapters. Support documents for the Transit Investment Plan are now: Transit Design and Performance Guidelines.

Regional Aviation System

The regional aviation system is composed of Minneapolis-St Paul International Airport, eight publicly owned regional airports, and two privately owned seaplane bases. These regional airports are owned and operated by the Metropolitan Airports Commission (MAC) and local communities.

Key Changes from the 2040 TPP

- The MAC completed an update to the Long-Term Comprehensive Plan (LTCP) for the Minneapolis-St Paul International Airport in 2024. This update moves the planning horizon to 2040 with new operation forecasts, anticipated noise impacts and capital project needs.
- The Aviation System Plan now identifies all facilities where aviation activity may take place in the region beyond regional airports. These facilities may be incorporated into and considered in local planning. Aviation appendix items of the TPP are now included as a single support document titled Aviation Supporting Information.

Regional Bicycle System

The regional bicycle system is identified through the Regional Bicycle Transportation Network (RBTN). The RBTN is made up of alignments and corridors. Corridors are shown where more specific alignments within those corridors have not yet been designated. The process used to develop the RBTN, the general principles and analysis factors used in its development, and studies done to analyze and update the network can be found in the *Bicycle Investment Plan*. The RBTN was established in the 2040 TPP with the intent of achieving the following goals:

- Establish an integrated and seamless network of on-street bikeways and off-road trails.
- Provide the vision for a “backbone” arterial network to serve daily bicycle trips by connecting regional destinations and local bicycle networks.
- Encourage cities, counties, park agencies, and the state to plan and implement future bikeways in support of the network vision.

The RBTN also incorporates regional bicycle barriers which are established in the TPP as the region’s most significant physical barriers to everyday bicycle travel and include freeways and expressways, railroad corridors, and secondary rivers and streams. They were developed and analyzed through the [Regional Bicycle Barrier Study](#), the map which displays these barriers can be found here: [Regional Bicycle Barriers Map](#).

Key Changes from 2040 TPP

- The RBTN has been refined and expanded since it was first implemented in the 2040 TPP. Requests from local governments to add and revise RBTN corridors and alignments prompted the [Regional Bicycle Guidelines and Measures Study](#) which developed specific measures used in reviewing update requests submitted by local agencies.
- The RBTN undergoes a formal update of alignments and corridors about every 2 years. The next formal update will take place in 2025 prior to the 2026 Regional Solicitation.

Regional Freight System

The regional freight system is composed of interconnecting transportation modes, some of which are unique to freight and some which overlap with passenger travel modes. There are five distinct modes of freight transportation: highway trucks, railroads, river barges, air freight, and pipelines. Combined with intermodal or warehouse/distribution terminals and supporting

infrastructure these modes of transportation comprise the regional freight system, known as the Metropolitan Freight System.

Key Changes from 2040 TPP

- E-commerce has become an increasingly important segment of freight movements and deliveries of goods to homes. The [Urban Freight Distribution Study](#) focused on e-commerce trends and impacts or opportunities of last-mile parcel deliveries in regional communities. The study notes that as urban deliveries become more common local communities are encouraged to create curb management policies, consider delivery needs in their complete streets planning and identify microhubs for last mile parcel distribution in addition to other recommendations.

Pedestrian System

The Council coordinates with local partners on pedestrian issues and trends, and aids in funding for pedestrian needs in a variety of ways.

Key Changes from 2040 TPP

- In previous TPPs pedestrian and bicycle travel were combined into one chapter and discussed together. The pedestrian system is now discussed and considered as an independent investment section of the 2050 TPP.
- To address rising pedestrian danger on our streets, the Council undertook a study to create the [Pedestrian Safety Action Plan](#), which was released in 2022. This plan includes analysis of crash trends in the region, recommendations of actions to take to increase pedestrian safety, and a weighted crash score on every road in the region to aid in future safety planning and project prioritization. Communities are encouraged to incorporate the plan's findings into the pedestrian elements of their comprehensive plans.

Travel Demand Management

Travel demand management (TDM) is the application of strategies, programs and policies to increase the efficiency of transportation systems by reducing travel demand, or redistributing this demand in space or in time. These initiatives are intended to inspire new travel habits to support infrastructure investments and a more efficient use of the transportation system by reducing the demand for personal vehicle trips.

Key Changes from 2040 TPP

- TDM has its own dedicated investment plan and discussion in the 2050 TPP, which is completely new to the plan. This section describes key partners in TDM in the region and identifies roles and responsibilities for these partners. As a result of this new investment plan, local communities are being asked to discuss how they include travel demand management strategies in their local plans, since some of them have relationships to land use controls and ordinances.
- The Council conducted the Regional Travel Demand Management Study that concluded with the [Regional Travel Demand Management Action Plan](#) in 2023. The Action Plan outlines a structure for a regional TDM program to implement and operationalize

strategies, programs and incentives that achieve reductions in single-occupant vehicle trips, vehicle-miles traveled, and greenhouse gas emissions through travel behavior change. The Action Plan is summarized in the 2050 TPP Travel Demand Management Investment Plan.

System Plan Considerations Affecting Your Community

Your community should consult the complete TPP in preparing its local comprehensive plan. In addition, your community should consult Imagine 2050 and the current version of the Metropolitan Council's Local Planning Handbook (LPH) for specific information needed in its comprehensive plan. Specific system plan considerations affecting your community are detailed below.

Regional Highway System

There are principal arterials located within your community:

- Interstate 35E
- Anoka CSAH 14
- Anoka CSAH 23

The 2050 TPP includes the following regional highway mobility improvements in your community as shown in the *Highway Investment Plan* Figures 8, 9, 10, 11:

- Funded or Planned Interchanges
 - I-35E at County Road J

Regional Transit System

Your community is located within the following Transit Market Areas: TMA 4 and TMA 5.

- TMA 4 is primarily Suburban Edge communities along with portions of Suburban communities. It is characterized by low population and job densities that do not support frequent local transit service and a spread out, often circuitous, street network that hinders efficient routing. Transit service in Market Area 4 is mainly peak-period express and commuter service at park-and-rides which can effectively gather the lower density transit demand into one spot. There are some suburban local routes, including suburb-to-suburb connectors, although service is typically very infrequent. Non-regular route transit, usually dial-a-ride, is generally available.
- TMA 5 is primarily rural communities including rural centers but also some suburban edge communities as well as freestanding town centers characterized by very low-density development or undeveloped land not well suited for regular-route transit service outside of limited peak-period express and commuter service.

Your community is located within the Transit Capital Levy District.

There are no transitways or high frequency bus routes in your community.

Regional Aviation System

All communities must include an aviation element in the transportation sections of their comprehensive plans. The degree of aviation planning and development considerations that need to be included in the comprehensive plan varies by community. Even those communities not impacted directly by an airport have a responsibility to include airspace protection in their comprehensive plan. The protection element should include potential hazards to air navigation including electronic interference. Local comprehensive plans can also begin to consider other aviation uses which do not take place at airports, like drones.

- Your community is within the influence area of Anoka-Blaine Airport and Surfside Seaplane Base. The LTCP for the Anoka-Blaine airport is shown in the Aviation Supporting Information document of the TPP. The full Anoka-Blaine LTCP can be found [here](#). The Anoka-Blaine LTCP has not been updated since the 2040 TPP was last updated in 2020 but is planned to be updated and amended into the TPP in 2027.
- Consideration and information for Surfside Seaplane Base should be updated in your community's 2050 Comprehensive Plan.

Communities influenced by this airport should review the LTCP to ensure that the updated comprehensive plan developed by the community remains consistent with the airport plans. Consult the LPH for specific comprehensive planning requirements and considerations such as airport zoning, noise and other environmental mitigation, airport development and economic impacts, ground access needs, infrastructure requirements, and general land use compatibility.

Local communities should also consider other facilities which may generate aviation activity, such as heliports or private air facilities. To see all aviation facilities in the region including those that are not defined as a part of the regional aviation system, refer to Figure 6 of the *Aviation System Plan*.

Regional Bicycle Planning

The RBTN is depicted on Figure 2 of the *Bicycle Investment Plan*. The network consists of Tier 1 and Tier 2 corridors and alignments. These are not intended to be the only bicycle facilities in the region, and local units should also include local network plans in their communities.

In your community there are one RBTN corridors and three alignments. Your community should incorporate the RBTN map within your local bicycle plan maps and use your comprehensive planning process to identify suitable alignments within and along the RBTN corridors for future incorporation into the TPP. In addition, agencies should plan their local on and off-road bikeway networks to connect to the designated Tier 1 and Tier 2 alignments, as well as any new network alignments within RBTN corridors to be proposed in local comprehensive plans. Bikeway projects that complete segments of, or connect to, the RBTN are given priority for federal transportation funds through the Transportation Advisory Board's biannual regional solicitation.

In your community there are one or more identified bicycle barrier crossings. To determine which regional bicycle barriers and prioritized barrier crossing improvement locations occur within your community, please review the [Regional Bicycle Barriers Online Map](#). The tiered crossing improvement locations are used as one alternative criterion in the Regional Solicitation to distribute federal transportation funds. Communities are encouraged to analyze

and address the need for new bicycle barrier crossings or improved bicycle facilities at existing barrier crossings.

Regional Freight Planning

The Council encourages all local governments to plan for freight movement in their communities. Trucks are the major mode of freight movement in the region and across the nation to distribute consumer goods as well as move manufactured goods and commodities, and they operate in every community.

The Metropolitan Freight System and the National Highway Freight Network are shown in Figures 1 and 5 of the *Freight Investment Plan*, respectively. There is a freight facility in your community: Interstate 35E. These networks and facilities should be incorporated into your local comprehensive plan. The local plan should also plan for compatible adjacent land uses, consider last-mile freight delivery needs and curb management strategies for local deliveries, if applicable. Refer to the *Urban Freight Distribution Study* for specific guidance on how to consider e-commerce and local deliveries on the local transportation system.

Other Transportation Policy Plan Considerations

Pedestrian Planning

The Council encourages local governments to address pedestrian needs for transportation in their local comprehensive plans. The necessary planning for pedestrians can be done within your community's comprehensive plan. An adopted pedestrian, active transportation, or multimodal plan can be included as an addendum to or in addition to the comprehensive plan. This planning should also include ensuring your local community has a current ADA self-evaluation that covers the public rights-of-way for transportation. Agencies with 50 or more employees must also have an adopted ADA transition plan, not just a self-evaluation.

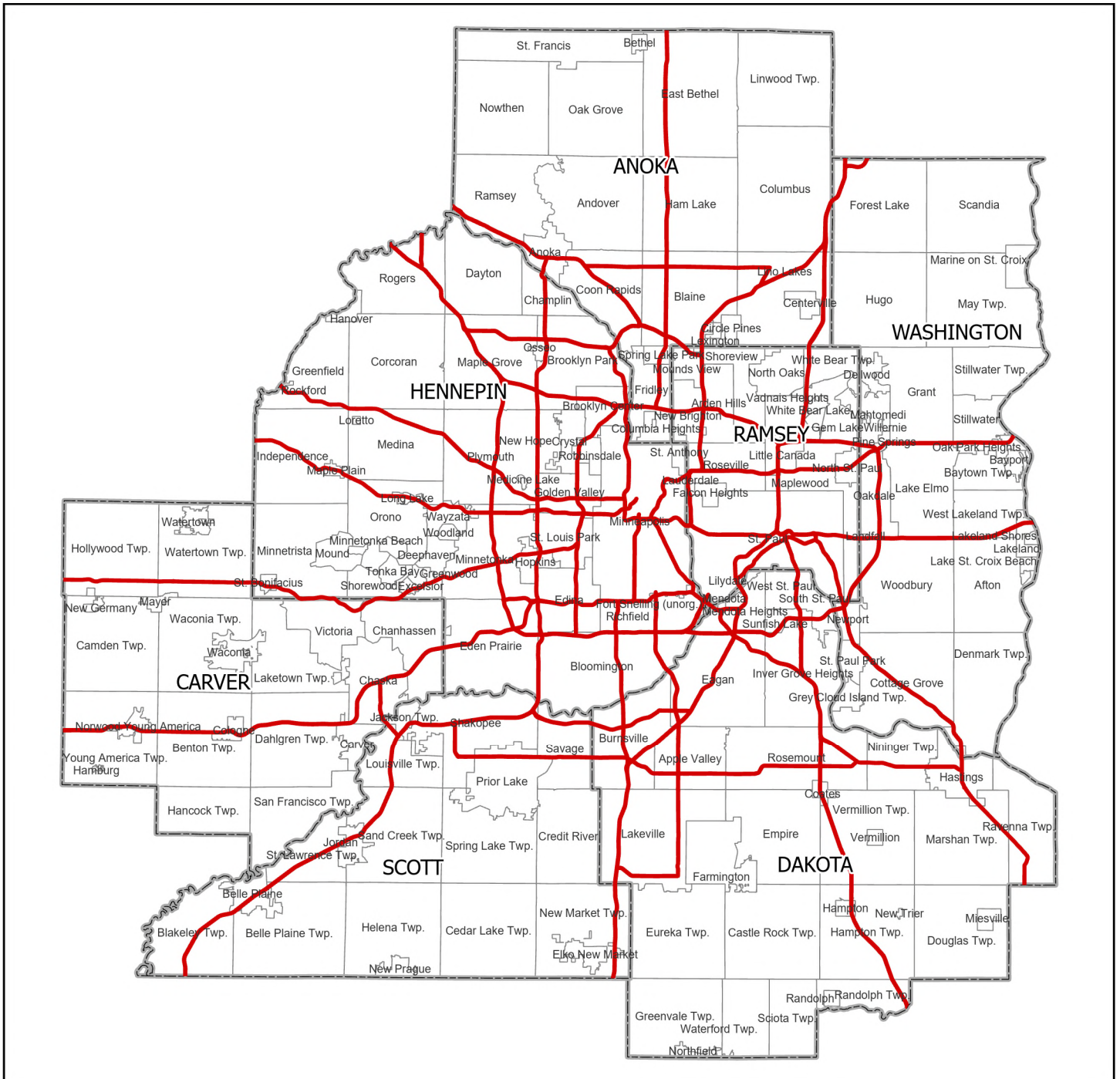
Travel Demand Management

The Council encourages local governments to address travel demand management needs for areas which experience traffic congestion. Your community is served by Anoka County TMO. Local governments should identify the transportation management organization which serves their community and describe any policies, ordinances, practices or programs in place which would aid in regional TDM work in their local comprehensive plans.

Greenhouse Gas Emissions

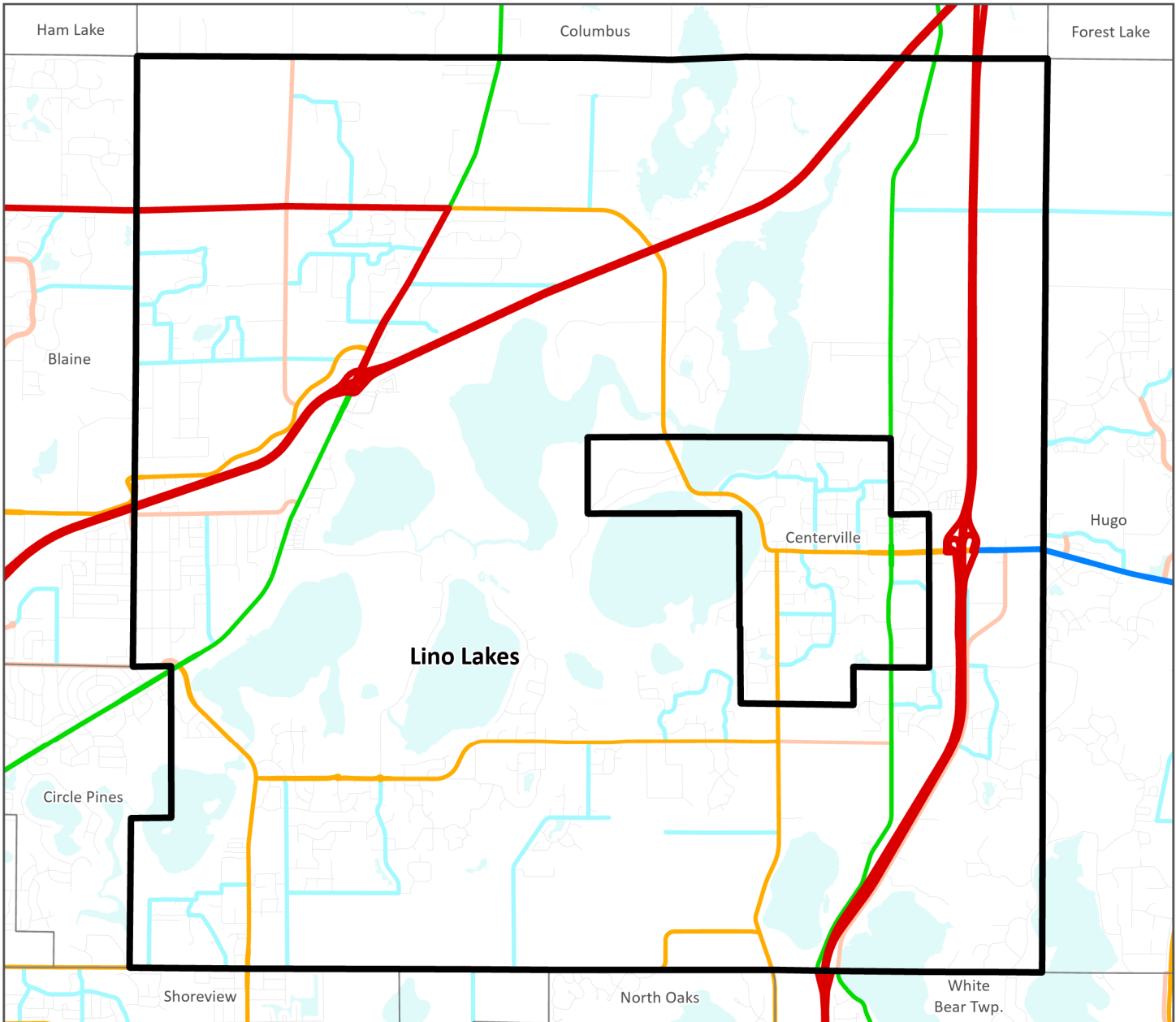
State law directs the Minnesota Department of Transportation to establish targets to meet greenhouse gas (GHG) emissions reduction for the seven-county metro to reach net-zero emissions in the transportation sector by 2050. Your community must include plans or strategies to limit GHG emissions on the surface transportation system in your comprehensive plan to meet the designated regional targets.

Principal Arterial Highways



— Principal Arterial Roads

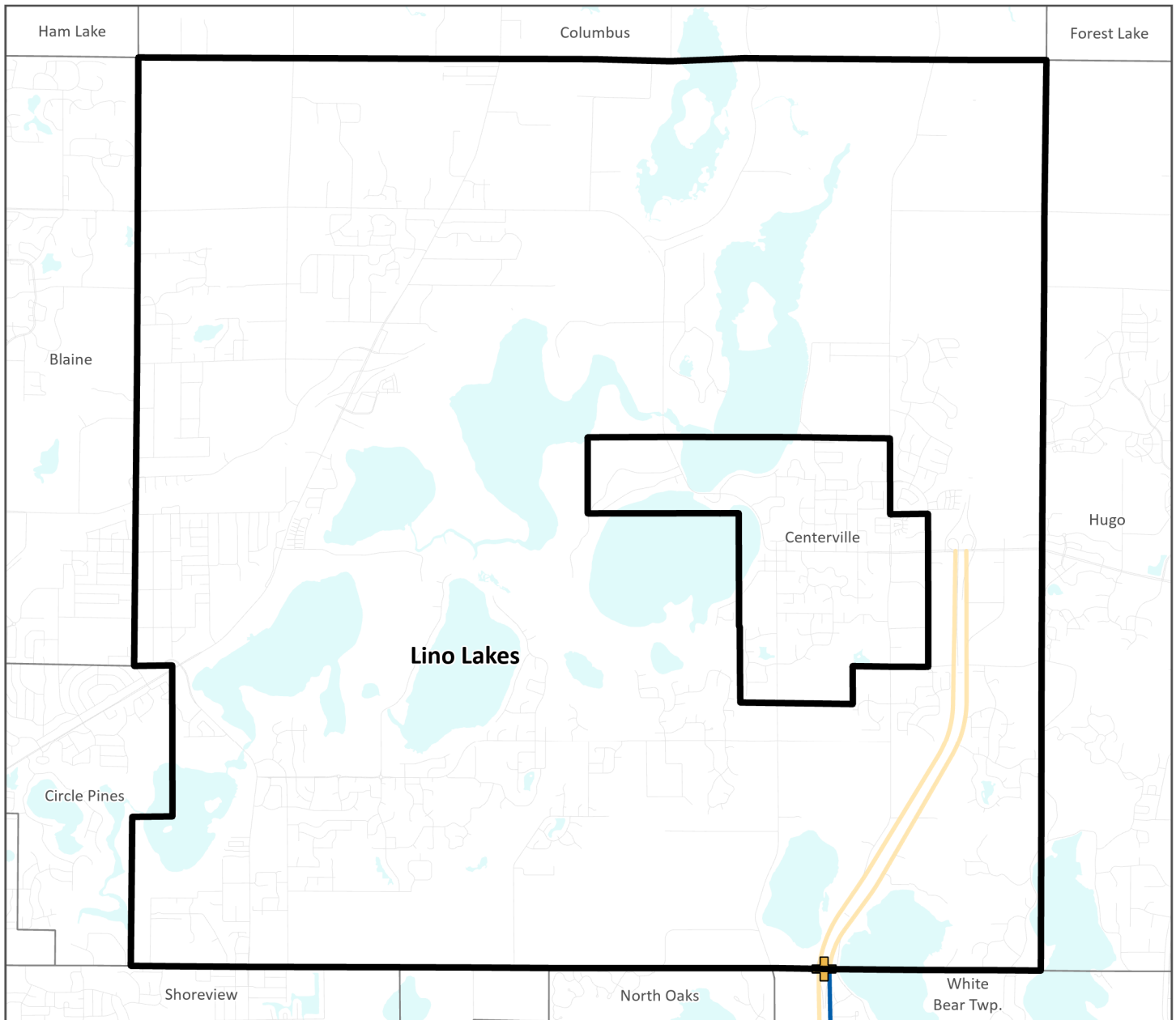
City of Lino Lakes Functional Class - All Roads












Functional Class

- Principal Arterial
- Minor Arterial Augmentor
- Minor Arterial Reliever
- Minor Arterial Expander
- Minor Arterial Connector
- Major Collector
- Minor Collector
- Local Roads

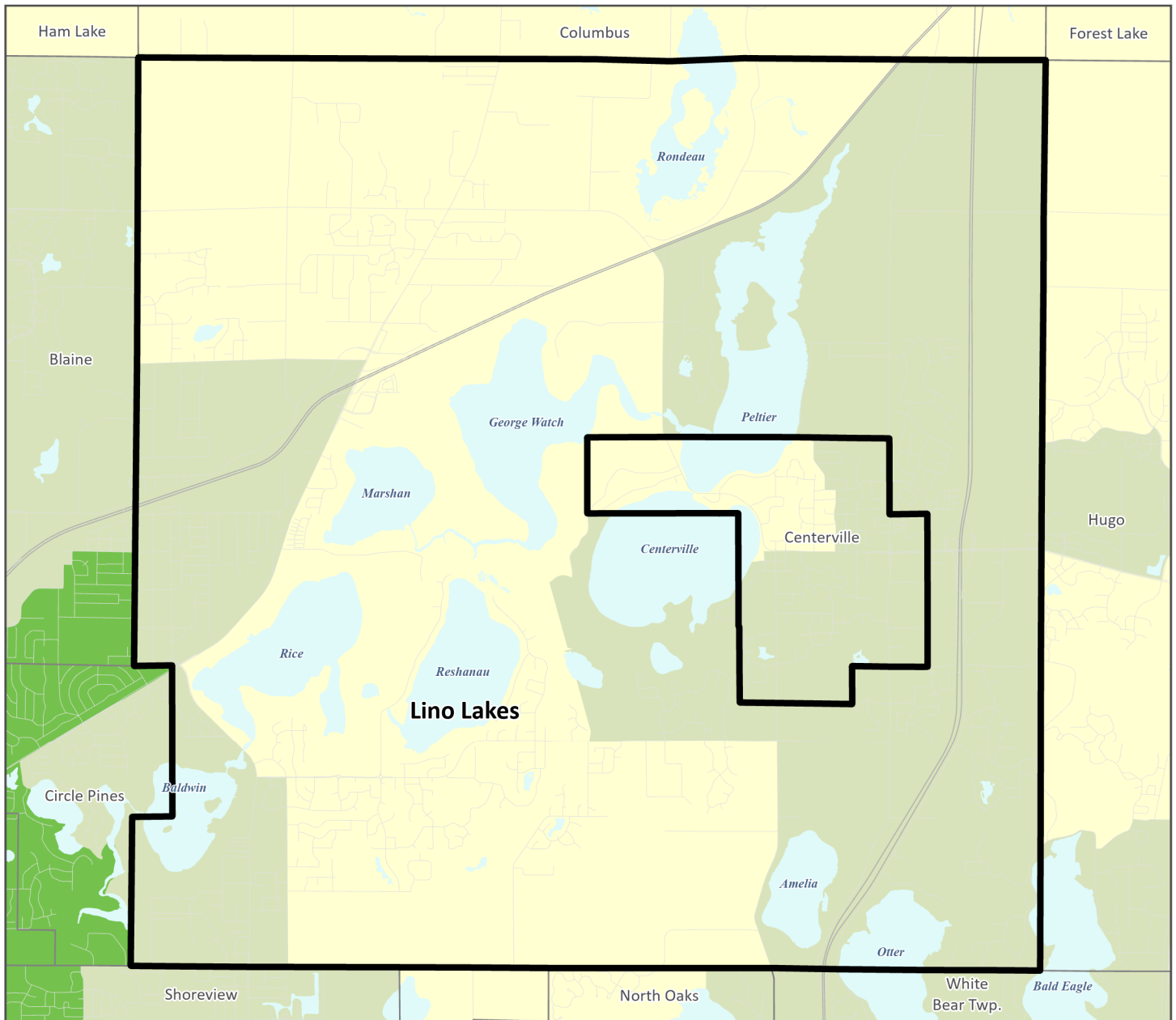
City of Lino Lakes Long Range Highway Projects



-  Interchanges
-  Targeted Regional Capacity
-  Safety
-  Spot Mobility

- E-ZPass System**
-  Existing E-ZPass System
 -  Under Construction E-ZPass
 -  Planned Managed Lanes
 -  Under Study Managed Lanes
 -  Vision Managed Lanes

City of Lino Lakes Transit Market Areas

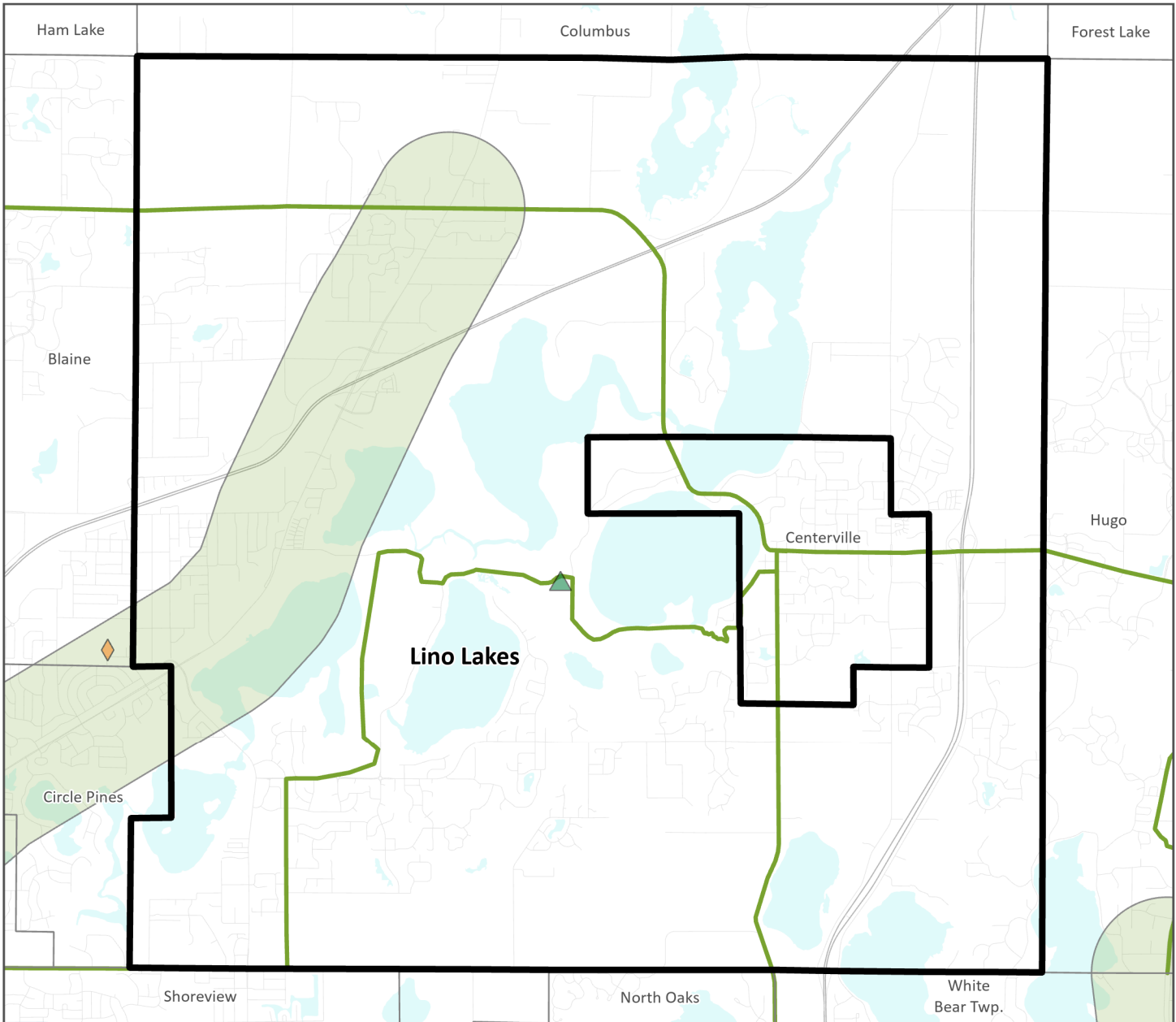


Transit Market Areas

- TMA 1
- TMA 2
- Emerging TMA 2
- TMA 3
- Emerging TMA 3
- TMA 4
- TMA 5

Freestanding Town Center

City of Lino Lakes Regional Bicycle Transportation Network



Alignments

- Tier 1
- Tier 2

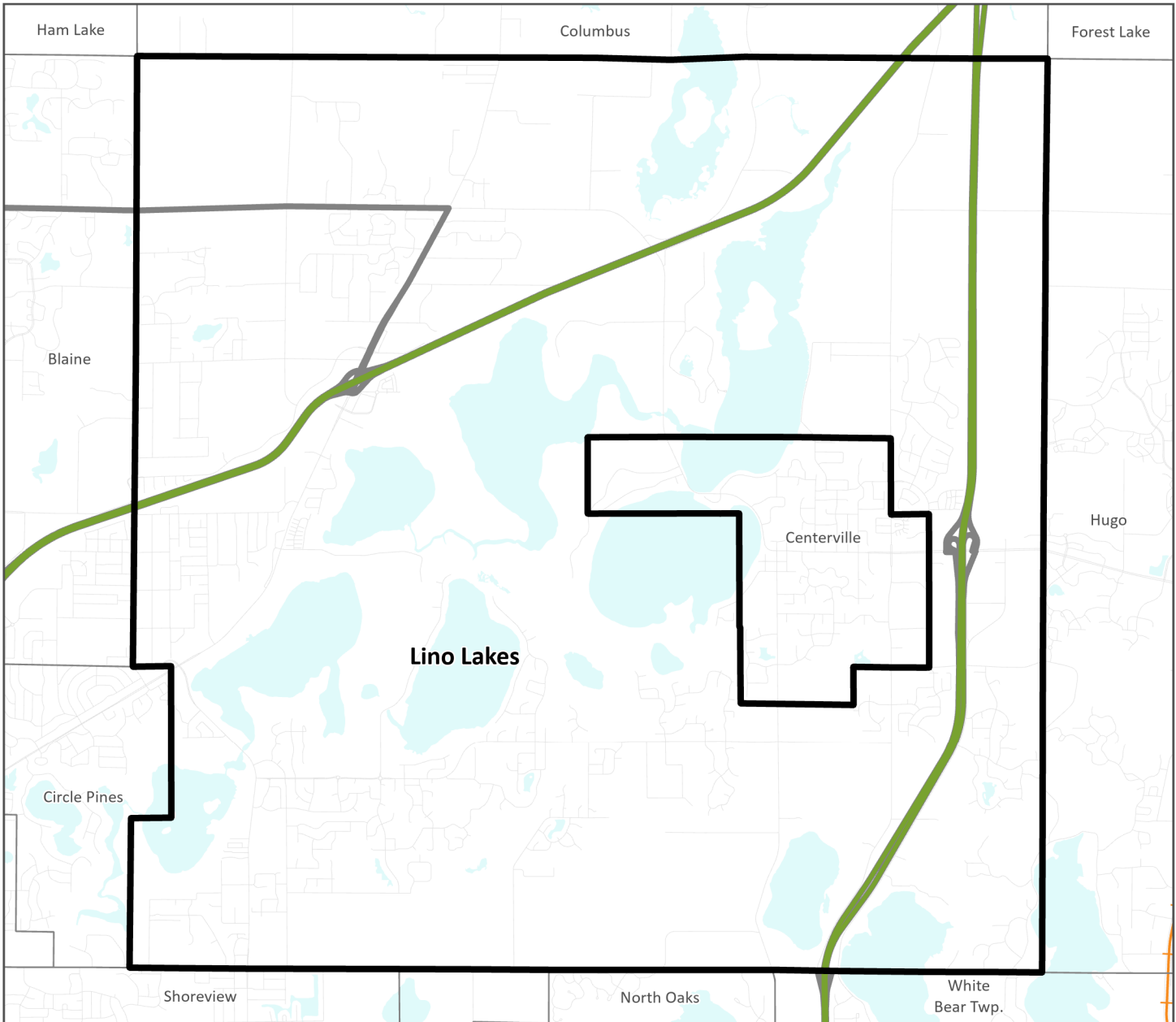
Corridors

- Tier 1
- Tier 2







Regional Destination Subgroup

- Metropolitan Job Centers (> 50,000 Jobs)
- Regional Job Centers (15,000 - 50,000 Jobs)
- Subregional Job Centers (7,000 - 15,000 jobs)
- ◆ Colleges & Universities (>2,000 Students)
- ◇ Large High Schools (> 2,000 Students)
- Major Sports & Entertainment Centers
- ▲ Regional Parks (> 400,000 visits/year)



City of Lino Lakes Metropolitan Freight System



Terminal Type

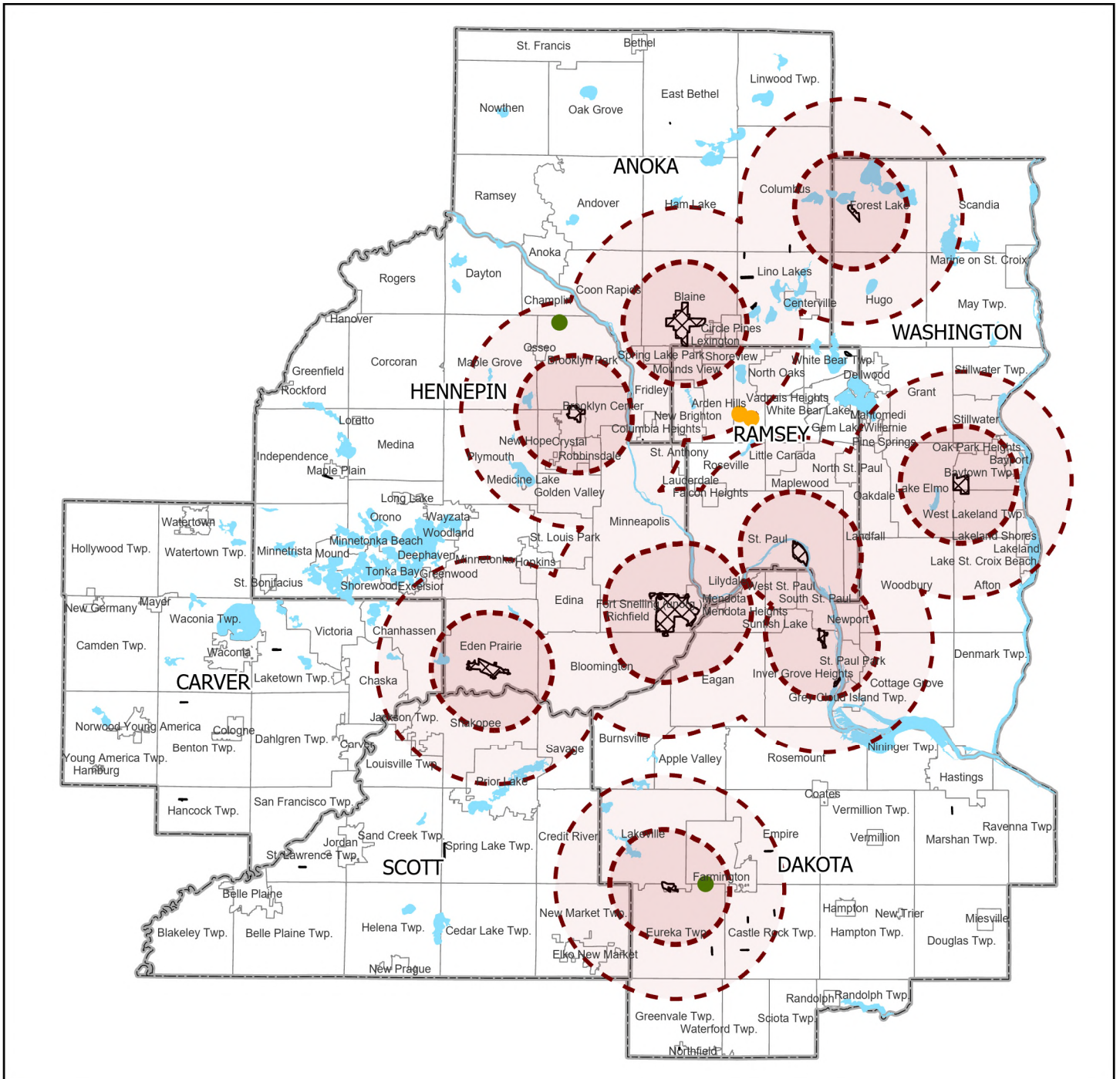
-  Airport Terminal
-  Pipeline
-  Rail Terminal - Container
-  Rail Terminal - Non Container
-  River Terminal
-  Trucking Terminal / Warehouse

Railroad Class

-  Class I Railroads
-  Class III Railroads

-  Primary Highway Freight System
-  Non-Primary Highway Freight System
-  Freight System Interstates
-  Critical Urban and Rural Freight Corridors

Regional Aviation System and Airport Influence Areas



- Tall Tower
- VOR
- 3 Mile Airport Influence Area - Noise, Zoning, Infrastructure
- 6 Mile Airport Influence Area - Landfills, Wind Towers
- Airport Land Area
- Aircraft Permitted Water

2025 WATER RESOURCES SYSTEM STATEMENT CITY OF LINO LAKES

Water Policy Plan Overview

The 2050 Water Policy Plan includes policies and strategies to achieve four objectives that support our regional goals:

- **CLIMATE:** The region's waters and water services are protected from and made resilient to the ongoing and future effects of climate change.
- **INVESTMENTS:** Water protection, planning, management, and infrastructure investments are optimized to ensure public and ecosystem health are fully protected now and for future generations.
- **HEALTH:** Natural waters, source waters, water services, and infrastructure are managed, restored, and enhanced to protect public and ecosystem health that ensures a high quality of life in the region.
- **EQUITY:** The benefits of clean and abundant water and water services are defined by local needs and environmental context, accessible, and justly shared by all residents and communities.

These objectives help to protect our region's groundwater and surface water, ensure the sustainability of water sources and utility service providers, support public and ecosystem health, and promote a thriving economy, sound development decisions, and a high quality of life for all who call this region home.

The Water Policy Plan strives to generate innovative actions, including water reuse, resource recovery, and nature-based approaches, that have multiple benefits at both the local and regional scales. Additionally, the plan includes policies and strategies to address the impacts of climate change, equitable access to water benefits, and water sector workforce concerns. The region has a strong history of water planning and management, and the 2050 Water Policy Plan continues this tradition. It progresses our organization and the region to ensure clean and abundant water for current and future generations.

Key Concepts

Adopted by the Metropolitan Council in February 2025, the 2050 Water Policy Plan - including the regional water policies, Wastewater System Plan, and the Metro Area Water Supply Plan - outline the conditions for regional wastewater service, surface water management, and water supply planning. Local comprehensive plans shall conform to these conditions. The Policy Plan incorporates the following changes:

Integrated Planning (next steps)

The Water Policy Plan is a guide for managing wastewater, water supply, surface waters, and groundwater. It takes an integrated approach to water planning and management from local to federal and Tribal levels, which helps to ensure a clean and plentiful water future. Water planning organizations need to work together to address issues that transcend political boundaries to achieve holistic water management.

Subregional Approach to Planning

The Met Council is committed to continuing and broadening the subregional approach to integrated water planning. Subregional groups will be expanded to include stakeholders from Tribal Nations, state agencies, county planning staff, municipal public works and planning staff, watershed organizations, non-governmental organizations, and others who want to participate in discussions about localized, integrated water concerns.

Water Equity

The Water Policy Plan prioritizes equitable access to clean water and water services, especially for historically marginalized communities. Reaching these outcomes requires initiatives such as prioritizing investments in overburdened communities, addressing historical harms, creating accessible information and communications, and including diverse perspectives of community members in water planning and management decisions.

Climate and Natural Systems

The Water Policy Plan includes climate and natural systems objectives, policies, and actions, which guide the Met Council and communities to employ approaches that lead to sustainable water and water services. This includes building water resiliency, limiting risks, and benefiting a growing and a thriving economy through convening partners, new tools and technologies, water conservation and protection efforts, and water planning and technical assistance.

Considerations Affecting Your Community

Regional Sewer Service

The treatment of wastewater is foundational to ensure public health and protect the region's natural environments. Reclamation of water and byproducts from the wastewater treatment process can help our region meet sustainability and climate goals. To operate and steward the regional wastewater system, it is essential for the Met Council to understand local conditions, identify current and future needs, and take innovative approaches to address the region's water sustainability challenges.

Local governments are required to submit both a wastewater plan element to their comprehensive plan and a comprehensive sewer plan describing service needs from the Met Council (Minnesota Statutes 473.513). A complete list of requirements for the comprehensive sewer plan, depending on wastewater service methods in your community, can be found in the Water Resources section of the [Local Planning Handbook](#).

Forecasts

The forecasts of population, households, employment, and wastewater flows for Lino Lakes can be found on your Community Page in the Local Planning Handbook. These forecasts are for sewer development. The sewer forecasts were estimated using Met Council's census block forecast data, individual and communal sewage treatment system data, Sewer Availability Charge (SAC) data, annual city reports, current trends, existing and future local wastewater service areas and other municipal information. The wastewater flows are based on historical wastewater flow data, future wastewater generation rates, and the sewer population and employment forecast data.

The Met Council will use these growth and wastewater flow forecasts to plan future regional wastewater conveyance and treatment system improvements needed to serve your community.

Lino Lakes, through its comprehensive planning process, must decide the location and staging of development, and then plan and design its local wastewater collection system to serve this development.

Wastewater Service

Metropolitan Council Environmental Services (Environmental Services) designs, constructs, and operates the regional wastewater system (Metropolitan Disposal System): a publicly owned system of regional sanitary sewers and water resource recovery facilities (wastewater treatment plants), for the conveyance, treatment and disposal of domestic waste, industrial waste and other waste from residential, commercial, institutional, and industrial users in the metropolitan area. The regional wastewater system currently serves 111 communities and through its work it protects public health, protects the environment, and fosters the economic growth of the seven-county Twin Cities Metropolitan Area.

A Regional Sanitary Sewer System map is provided to assist in the completion of your community's comprehensive sewer plan. The map shows Environmental Services' wastewater infrastructure located within your community's boundaries, if your community is provided regional wastewater service.

A complete list of items required for inclusion in your community's comprehensive sewer plan, depending on wastewater service methods in your community, can be found in the Local Planning Handbook. A comprehensive plan will be found incomplete for review if all submittal requirements are not met.

If your community has multiple methods of wastewater service (including regional service, local (municipal) wastewater treatment system, private communal systems, and/or subsurface sewage treatment systems), review the information in each corresponding section and refer to the Local Planning Handbook for a complete list of requirements that must be included in the comprehensive sewer plan for these systems. We strongly encourage you to include this information in a water chapter of your comprehensive plan.

Areas Served by the Regional System

Current wastewater treatment services are provided to the City by Environmental Services. Wastewater generated within the City is conveyed by Met Council Interceptors 9106, 9708, and 8361 with treatment at the Metropolitan Water Resource Recovery Facility in St. Paul.

Environmental Services has made investments in the area with the Blaine Relief Interceptor to support the growth in the western portion of the City.

Regional Inflow and Infiltration Program

The Met Council is continuing implementation of its inflow and infiltration (I/I) reduction program and will continue to establish I/I goals for all communities discharging wastewater to the regional wastewater system. Communities that have excessive I/I in their sanitary sewer systems will be required to eliminate the excessive I/I. Those communities will be required to submit a work plan that details work activities to identify and eliminate I/I sources from both municipal and private sources. The Met Council will not design future regional sanitary sewer improvements or water resource recovery facilities to handle peak hourly flows in excess of the allowable rate for your community. Increases in service may be limited in communities that do not demonstrate progress in reducing excess I/I.

Two grant programs are currently available for communities with excessive I/I in their sanitary sewer systems: The Municipal Inflow and Infiltration Grant Program and the Private Property Inflow and Infiltration Grant Program. It is recommended that communities review these programs and determine if participation fits into their I/I work plan. The Municipal Inflow and Infiltration Grant Program is based on annual legislative action and funding amount and availability may vary year to year.

Areas Served by Local Wastewater Treatment System

The requirements of this element do not apply because Lino Lakes does not have a local wastewater treatment system.

Areas Served by Private Communal Treatment Systems and/or Subsurface Sewage Treatment Systems

The Met Council's position is that private communal wastewater treatment systems should only be permitted in areas not programmed for regional sewer service in the foreseeable future and they are provided for in a community's comprehensive plan. The community is responsible for permitting all private communal or cluster wastewater treatment systems consistent with current Minnesota Pollution Control Agency standards (Minnesota Rules Chapter 7080-7083). The Met Council will not provide financial support to assist communities if these systems fail.

Communities with individual subsurface sewage treatment systems (SSTS), commonly known as septic systems, must adopt a management program consistent with current Minnesota Pollution Control Agency regulations (Minnesota Rules Chapter 7080-7083). A description of the management plan and current SSTS ordinance must be included in the community's comprehensive sewer plan.

Surface Water Management

The metro region consists of hundreds of miles of rivers, streams, thousands of acres of wetlands, and nearly a thousand lakes. These surface waters define our region. They are where we play, exercise, find peace, and celebrate with friends and family. They support the region's ecosystems and biodiversity. They provide drinking water for the region's residents and energy for industry. They are critical transportation corridors and places to recreate, fueling local economies. Yet these waters are threatened by complex issues like ongoing pollution stress, climate change, and unsustainable development pressures. Comprehensive community planning includes surface water planning to ensure the region's residents, businesses, and ecosystems can benefit from clean and abundant water.

In 1995, Minnesota Statutes Section 473.859, subd. 2 was amended to make the local water plan (often referred to as local surface water management plans) required by Section 103B.235 a part of the land use plan of the local comprehensive plan. Minnesota Rules Chapter 8410, updated in July of 2015, includes the requirements for local water management plans. All communities in the metro region must update their local water plan between Jan. 1, 2027 and Dec. 31, 2028. This means that Lino Lakes must update its local water plan as part of the comprehensive plan update. The community's updated local water plan should be submitted to the Met Council for its review concurrent with the review by the local watershed management organizations.

The Surface Water Features map shows the watershed management organizations, Rice Creek WD and Vadnais Lake Area WMO, that have jurisdiction in Lino Lakes.

Failure to have an updated local water plan approved by your watershed management organizations will result in the comprehensive plan being incomplete for review. Local water plans shall be submitted to the Met Council for review in the timeframe described above, comments are sent from the Met Council to the appropriate watersheds for inclusion in their review and approval of the plan, and finally the plan is approved by the appropriate watersheds.

Local water plans must meet the requirements for local water plans in Minnesota Statutes, section 103B.235 and Minnesota Rules Chapter 8410. In general, local water plans need to include a summary of the priorities and problems in the community; structural, nonstructural and programmatic actions to take to address the priorities and problems; and clearly identified funding mechanisms to fix the problems.

More detailed guidance for the local water plans can be found in Appendix A of the [2050 Water Policy Plan](#) and in the Met Council's current Local Planning Handbook.

Priority Waters List

The Met Council updated its Priority Waters List (formerly Priority Lakes List) in July 2022. This new version includes rivers, lakes, and streams. With more than 950 lakes and hundreds of miles of rivers and streams in the region, waterbodies needed to be prioritized to adequately dedicate staff and financial resources. The Met Council uses the Priority Waters List to focus

its limited resources. The list is also used in the environmental review process. The Surface Water Features map and Priority Waters List table show the priority waters for Lino Lakes.

When using this Priority Waters List, for projects near a specific waterbody, we recommend you connect with local residents to understand how they value and interact with the waterbody. The Twin Cities region is home to many diverse communities with different cultural and personal relationships to water, so it's important to incorporate those perspectives in addition to the Priority Waters List when working on local-scale projects. Communities should identify the Priority Waters and the projects and/or programs that will protect or restore these waters.

The Water Contamination and Impaired Waters map includes any water bodies that are on the Minnesota Pollution Control Agency's 303d Impaired Waters List.

Table of Priority Waters for City of Lino Lakes

Waterbody Type	Name	DNR Lake ID	DNR Kittle Number
Lake	Amelia	02001400	---
Lake	Bald Eagle	62000200	---
Lake	Baldwin	02001300	---
Lake	Centerville	02000600	---
Lake	George Watch	02000500	---
Lake	Marshan	02000700	---
Lake	Otter	02000300	---
Lake	Peltier	02000400	---
Lake	Rice	02000800	---
Lake	Rondeau	02001500	---
River/stream	Rice Creek	---	M-059

Water Supply

Water supply is not a regional system. However, water supply information is required for local comprehensive plan updates to meet statutory requirements and for consistency with regional policy.

To ensure that there is a safe and plentiful supply of water—for a wide range of residential, commercial, institutional, industrial, recreational, and other purposes—it is important to make sure local water supply sources, infrastructure, and planned investments are aligned with planned land use changes.

The Met Council recognizes the local responsibility and authority for water supply planning. However, a regional perspective is also valuable, because the effects of local water supply decisions do not stop at community boundaries. The Met Council provides regional planning, guidance, and resources to support communities and help safeguard our shared water resources.

Water supply plan-related requirements generally include:

- Clearly identifying the locations of water sources and amount of water that is currently used and is planned to be used for things like agriculture, homes, businesses,

industries, and other public and private purposes. This includes areas that affect those water sources, such as source water protection areas.

- Creating a program for how to implement local rules and regulations about water supply, including when and how these rules will be developed, adopted, and administered.

Communities served by a municipal community public water supply system must fulfill part of these requirements by attaching a local water supply plan approved by the Minnesota Department of Natural Resources as an appendix to the comprehensive plan.

The Water Supply Considerations map illustrates some key content for your community, including Drinking Water Supply Management Areas, Special Well and Boring Construction Areas, and Priority Waters qualifying as drinking water sources.

We strongly encourage you to include any required information that isn't in the local water supply plan—such as source water protection and privately-owned wells—in a water chapter of your comprehensive plan.

A customized checklist of minimum requirements for your community is included in the Local Planning Handbook, along with resources to help you meet and go beyond minimum requirements.

Source Water Protection

Your comprehensive plan should consider water use (including water supply sources) as part of land use planning, to promote land use practices and development decisions that protect public health for your community and the region. Include information about the location of both groundwater and surface water source water protection areas and their vulnerability for all community public drinking water source(s) within your community's borders and associated contaminant threats. Also include a commitment to collaborate with neighbors on source water protection, when applicable.

Privately-Owned Wells and Nonmunicipal Public Water Supply Systems

Your comprehensive plan should include information about the current and planned use and management strategies for privately-owned wells and nonmunicipal public water supply systems, because people, institutions, and businesses in your community use those sources for a wide range of agricultural, residential, commercial, industrial and/or other nonmunicipal purposes.

If a new municipal community public water supply system is planned by 2050, a water chapter of the updated comprehensive plan should include details about the planned system.

Municipal Community Public Water Supply Systems

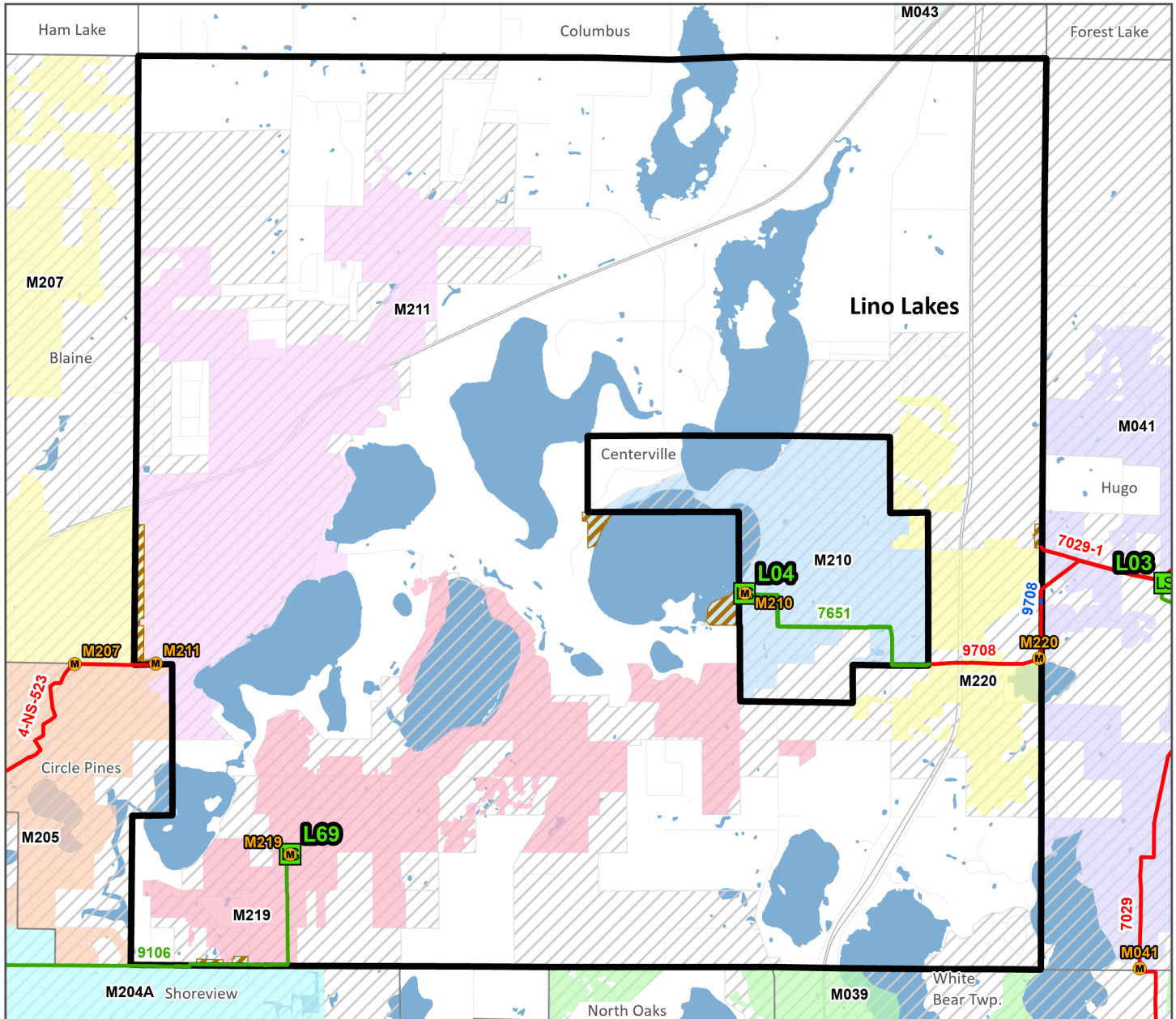
Water Supply System Information

Because people, institutions, and businesses in your community get water through a municipal community public water supply system, you must include information about that system and an implementation program in your comprehensive plans, to demonstrate the availability of clean,

safe drinking water to meet projected water demand consistent with the Met Council's forecasts.

The Minnesota Department of Natural Resources (DNR)-approved local water supply plan for the municipal public water supplier providing service to your community must be attached as an appendix to the comprehensive plan. To ensure that the DNR-approved local water supply plan is consistent with regional policies and is compatible with adjacent and affected governmental units, you should provide the Met Council and adjacent and affected jurisdictions with the opportunity to review and comment on your draft local water supply plan update. Failure to include an updated local water supply plan approved by the DNR will result in the comprehensive plan being incomplete for review until the required plan is submitted to the Met Council.

City of Lino Lakes Regional Sanitary Sewer System



- Meters
- Lift Station

Regional Sewer

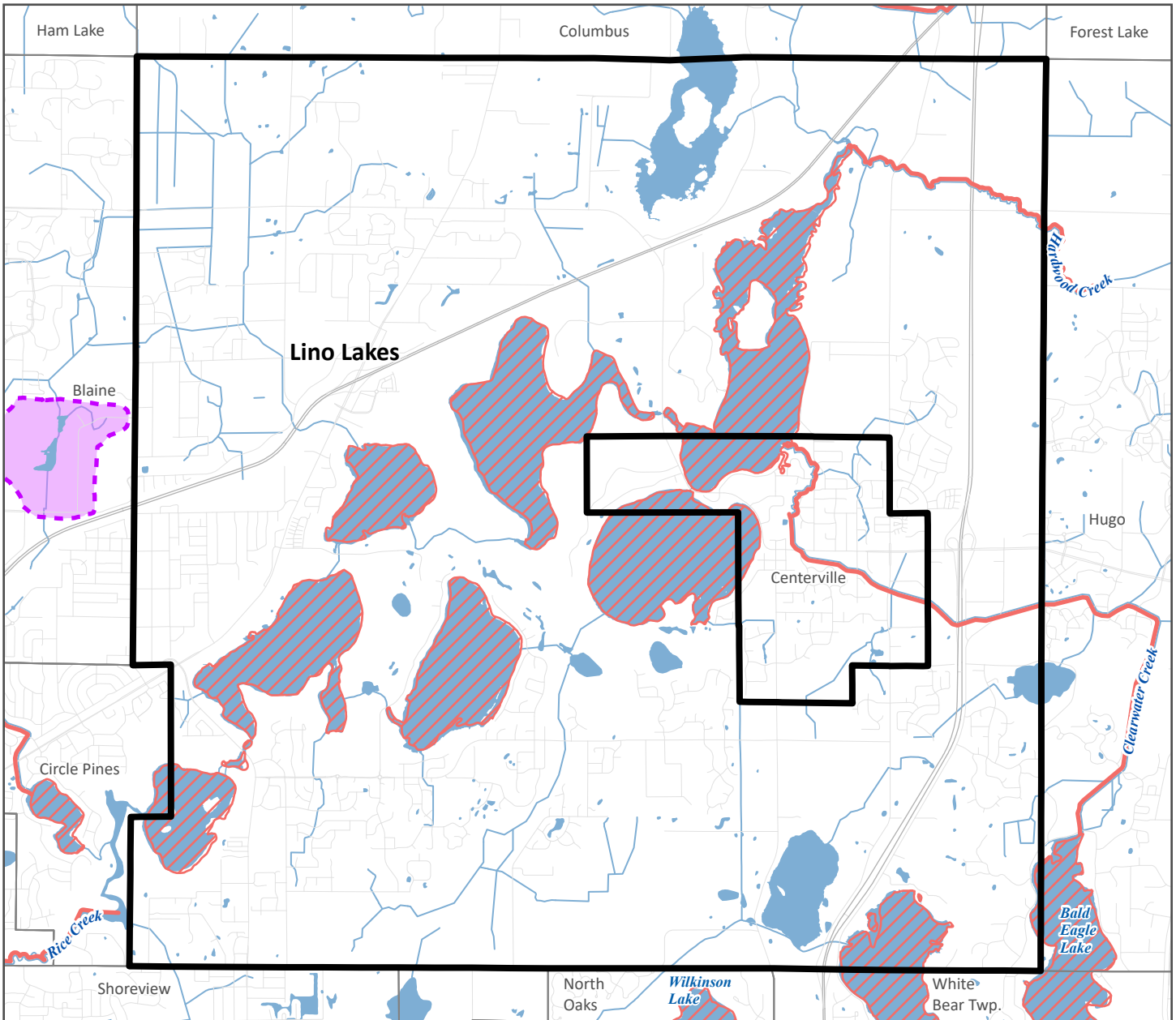
- Gravity
- Forcemain
- Siphon

- Areas of Unmetered Flow from the Community
- 2050 MUSA




Sewer Service Areas

To Meter M###	Not Served
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City of Lino Lakes Water Contamination and Impaired Waters



Minnesota Groundwater Contamination Atlas

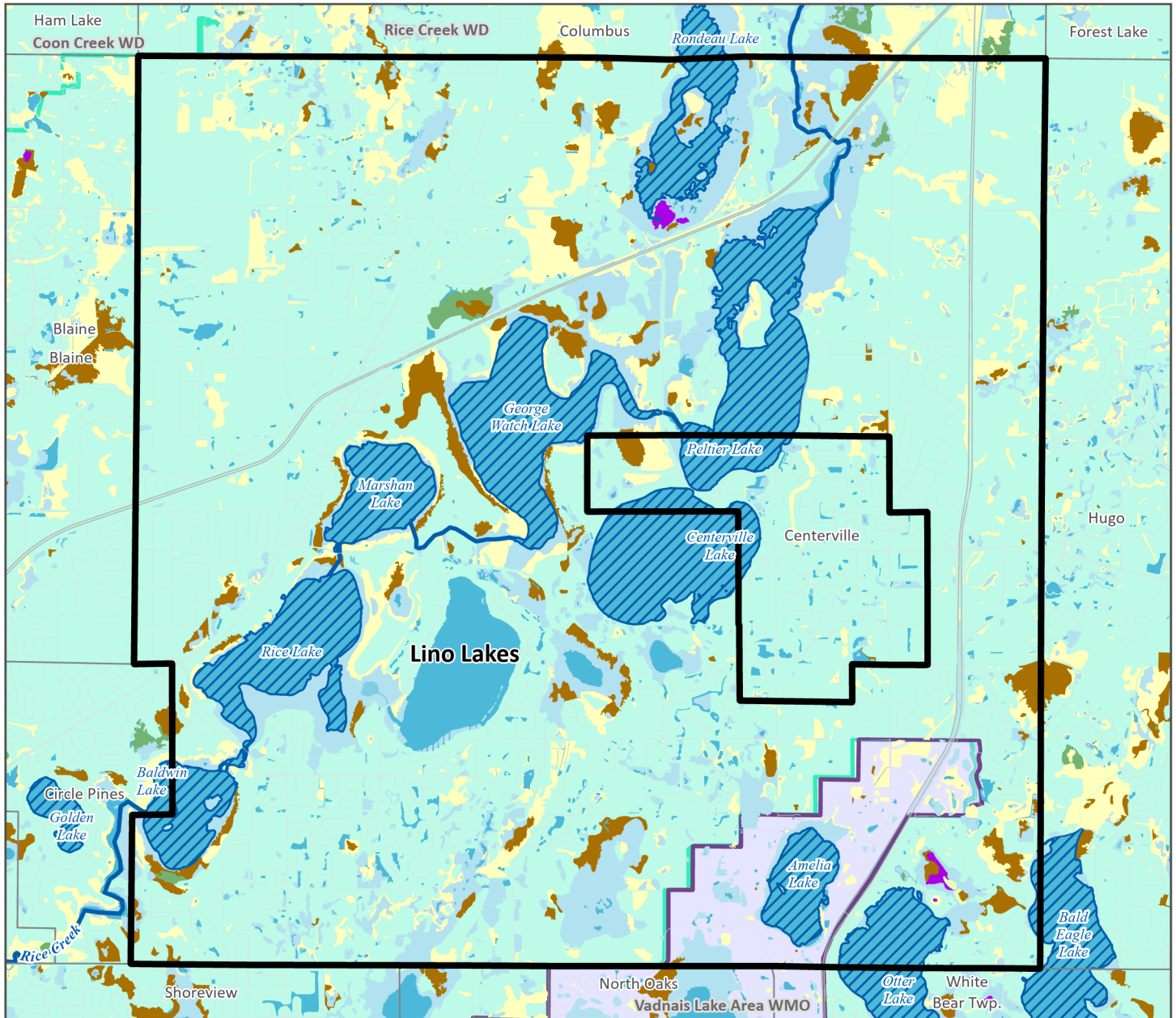
-  High Confidence Boundary
-  Low Confidence Boundary
-  Groundwater area of concern *

Minnesota's 2024 Impaired Waters List

-  Impaired Rivers or Streams
-  Impaired Lake

* darker shades of purple represent multiple overlapping areas of concern



City of Lino Lakes Surface Water Features





Watershed Management Type

-  County Administered Planning
-  Watershed District (WD)
-  Watershed Management Organization (WMO)

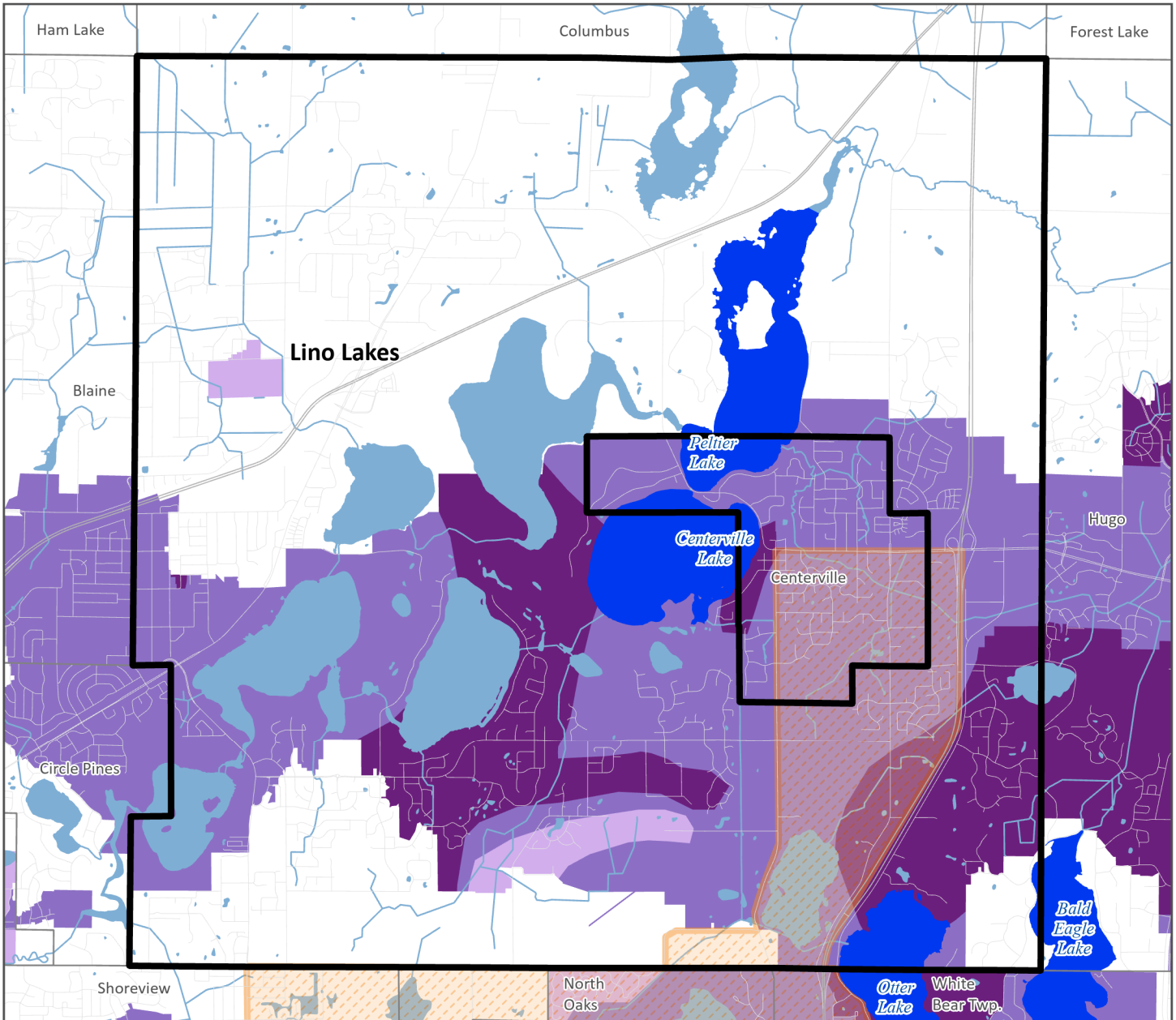
Wetland Type

-  Seasonally Flooded Basin or Flat
-  Wet Meadow
-  Shallow Marsh
-  Deep Marsh
-  Shallow Open Water
-  Shrub Swamp
-  Wooded Swamp
-  Bog

Priority Water

-  Priority River or Stream
-  Priority Lake

City of Lino Lakes Water Supply Considerations



Drinking Water Supply Management Areas for Groundwater (DWSMA)

- High Vulnerability
- Moderate Vulnerability
- Low Vulnerability

Drinking Water Supply Management Areas for Surface Water (DWSMA)

- Surface Water DWSMA Priority Area A
- Special Well and Boring Construction Area
- Priority river or stream qualifying as a drinking water source
- Priority lake qualifying as a drinking water source

REGIONAL PARKS AND TRAILS SYSTEM STATEMENT CITY OF LINO LAKES

Overview of the Regional Parks and Trails System

The Regional Parks and Trails System includes 66 regional parks, park reserves, and special features, plus more than 487 miles of regional trails that showcase the unique landscapes of the region and provide year-round recreation. The Regional Parks and Trails System is well-loved by our region's residents and attracted over 69 million visits in 2023.

The organizational structure of the Regional Parks and Trails System is unique, built upon a strong partnership between the Met Council and the ten regional park implementing agencies that own and operate Regional Parks and Trails System units. The regional park implementing agencies are:

Anoka County	Ramsey County
City of Bloomington	City of Saint Paul
Carver County	Scott County
Dakota County	Three Rivers Park District
Minneapolis Park and Recreation Board	Washington County

The 2050 Regional Parks and Trails Policy Plan advances the Imagine 2050 regional goals and core values including Equity, Leadership, Accountability, and Stewardship by striving to:

- **Foster equity and belonging:** Connect people with nature, community, and cultural landscapes to better support their physical, mental, and emotional well-being
- **Take care of what we have:** Reinvest in existing regional parks and trails to maintain and enhance visitor experiences
- **Protect and restore:** Protect and restore natural systems to safeguard the well-being of all living things
- **Adapt and mitigate:** Increase the region's resilience to climate change through land stewardship practices that mitigate greenhouse gases and adapt to future climates
- **Meet future needs:** Meet the growing demand for regional parks and trails through strategic and timely land acquisition and development.

Key Concepts in the 2050 Regional Parks and Trails Policy Plan

The 2050 Regional Parks and Trails Policy Plan includes the following policies, each with specific associated strategies:

- **System Plan policy:** Maintain a robust and current set of data, maps, plans, processes, and applications to support regional parks and trails planning.

- **Natural Systems policy:** Identify lands with high-quality natural features and/or with high restoration potential that are desirable for Regional Parks and Trails System activities and put these lands in a protected status so they will be available for recreational uses and preservation purposes in perpetuity.
- **Climate Resilience policy:** Adapt and enhance the Regional Parks and Trails System to promote resilience to climate change, including the mitigation of greenhouse gas emissions.
- **Planning policy:** Promote long-range planning and help provide integrated resource planning across jurisdictions in order to create a seamless system that connects everyone to the outdoors.
- **System Protection policy:** Protect public investments in acquisition and development by assuring that every component in the system is able to fully carry out its designated role.
- **Recreation, Facilities, and Programming policy:** Foster a sense of belonging by providing a wide spectrum of leisure and play opportunities while connecting people, places, and the natural world.
- **Finance policy:** The Met Council administers, provides financial oversight, and collaborates with a range of partners to fund the ten regional park implementing agencies in support of the Regional Parks and Trails System.

The 2050 Regional Parks and Trails Policy Plan is the metropolitan system plan for regional recreation open space with which local comprehensive plans must conform. This system statement highlights the elements of the system plan which apply specifically to your community. Find the complete text of the 2050 Regional Parks and Trails Policy Plan at the following url:

<https://imagine2050.metrocouncil.org/chapters/regional-parks-and-trails>.

Key Changes in the 2050 Regional Parks and Trails Policy Plan

Adopted by the Met Council in February 2025, the 2050 Regional Parks and Trails Policy Plan incorporates the following changes:

- Unit name changes
 - Anoka County
 - “Northwest Search Area” changed to “Sugar Hills Search Area”
 - Dakota County
 - “Lebanon Hills-Big Rivers Greenway Trail Search Corridor” changed to “Lebanon Hills-Minnesota River Greenway Trail Search Corridor”
 - “Lebanon Hills-Mendota Greenway Regional Trail, Highway 62 segment” changed to “Lebanon Hills Greenway Regional Trail”
 - Scott County
 - “Scott West Regional Trail” changed to “Big Woods Regional Trail”
 - “Minnesota River Bluffs Extension and Scott County Connection Regional Trail” changed to “Merriam Junction Regional Trail”
 - “Louisville Trail Search Corridor” changed to “Merriam Junction Regional Trail”
 - Portion of “Southern Scott Trail Search Corridor” changed to “Shallow Waters Regional Trail”

- Trail refinements
 - Three Rivers Park District's Dakota Rail Regional Trail Search Corridor Extension moving the trail terminus from Highway 494 to Minnetonka City Hall
 - Three Rivers Park District is transferring a portion of the Lake Minnetonka Regional Trail to Carver County
- Future 2028/2029 system additions process candidates
 - Ramsey County's Rice Creek North Regional Trail Boundary Adjustment adding 2,407 acres to the current 792 acres at the former Twin Cities Army Ammunition Plant
 - Three Rivers Park District's 5.5-mile West Minnehaha Creek Trail Corridor Study Area
 - Dakota County's 58-acre Thompson County Park Study Area in West St. Paul serving the outdoor recreational needs of the more than 50,000 residents who live in the northern portion of Dakota County

The 2050 Regional Parks and Trails System Plan Map is depicted in Figure 1. Lino Lakes should consult the complete 2050 Regional Parks and Trails Policy Plan in preparing its local comprehensive plan. In addition, Lino Lakes should consult Imagine 2050 and the current version of the Met Council's Local Planning Handbook for specific comprehensive plan requirements.

2050 Regional Parks and Trails System Units

The Regional Parks and Trails System comprises four main types of units: regional parks, park reserves, special features and regional trails.

- **Regional parks** contain a diversity of natural features, either naturally occurring or restored, and are typically 200-500 acres in size. Regional parks accommodate a variety of outdoor recreation activities. In 2024, a total of 46 regional parks are open to the public.
- **Park reserves**, like regional parks, provide for a diversity of outdoor recreation activities. One major distinguishing feature is that the minimum size for a park reserve is 1,000 acres. Additionally, regional park implementing agencies are required to manage at least 80% of the park reserve as natural lands that protect the ecological functions of the native landscape. As of 2024, a total of 12 park reserves were open to the public.
- **Special features** provide opportunities not generally found in the regional parks, park reserves, or trail corridors. Special features often require a unique managing or programming effort. As of 2024, there are eight special features open to the public.
- **Regional trails:** The Met Council has defined two major types of trails to serve the region: destination or greenway trails and linking trails. Destination or greenway trails typically follow along corridors with high-quality natural features that make the trail itself a destination. Linking trails are predominately intended to provide connections between various Regional Parks and Trails System units. As of 2024, 56 regional trails totaling approximately 487 miles were open for public use.

2050 Regional Parks and Trails System Components

The 2050 Regional Parks and Trails Policy Plan identifies five components which together comprise the vision for the Regional Parks and Trails System in 2050, as described below.

- **Existing Regional Parks and Trails System facilities** are open for public use and include land that is owned by regional park implementing agencies. They may include inholding parcels within the boundaries of these parks and trail corridors that have not yet been acquired. Existing regional trails may include planned segments that will be developed in the future.
- **Planned Regional Parks and Trails System facilities (not yet open to the public)** have a Council-approved long-range plan and may be in stages of acquisition and development but are not yet open for public use.
- **Regional Parks and Trails System boundary adjustments** include general areas identified as potential additions to existing Regional Parks and Trails System facilities to add recreational opportunities or protect natural resources. Specific adjustments to park or trail corridor boundaries have not yet been planned.
- **Regional Park and Special Feature search areas** include general areas for future regional parks and special features to meet the recreational needs of the region by 2050 where the regional park boundary has not yet been planned.
- **Regional trail search corridors** include proposed regional trails to provide connections between Regional Parks and Trails System facilities where the trail alignment has not yet been planned.

System Plan Considerations Affecting Your Community

The following Regional Parks and Trails System Components within Lino Lakes are identified in the 2050 Regional Parks and Trails Policy Plan:

Regional Parks, Park Reserves, and Special Features

- **Rice Creek Chain of Lakes Park Reserve:** This is an existing regional park reserve with an established boundary. Rice Creek Chain of Lakes Park Reserve, at nearly 5,500 acres, is located in the cities of Columbus, Lino Lakes, and Centerville. It includes significant native wildlife habitat, archeological sites, and family-friendly amenities such as a playground and a beach on Centerville Lake. The regional park boundary as shown in Figure 2 should be acknowledged in the comprehensive plan.

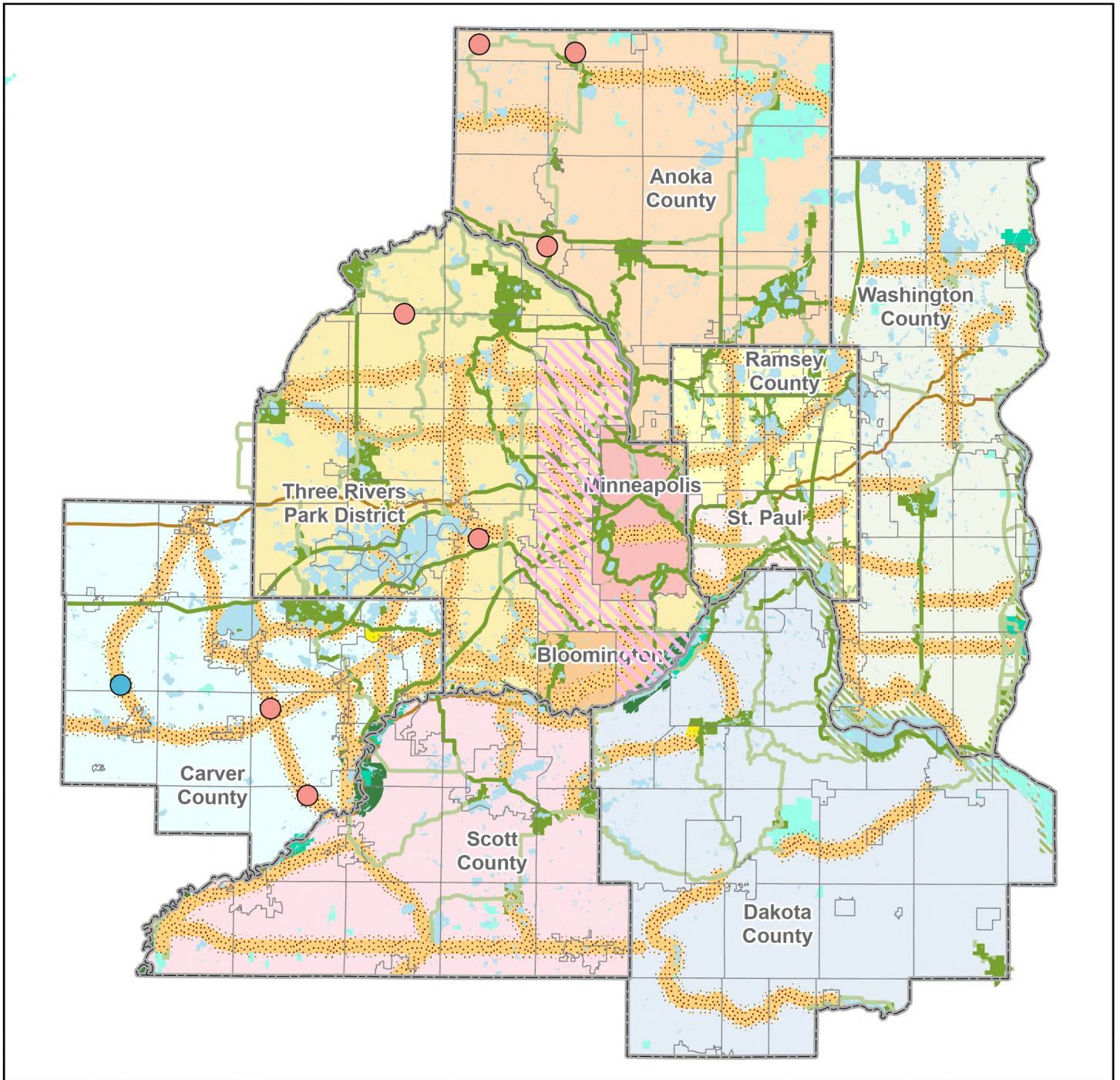
Regional Trails

- **Bunker Hills-Chain of Lakes Regional Trail:** This is a regional trail that includes segments that are open to the public as well as planned segments that will be developed in the future. The regional trail travels through Blaine, Lino Lakes, and Centerville as it connects Bunker Hills Regional Park, Rice Creek North Regional Trail, Rice Creek Chain of Lakes Park Reserve, and Central Anoka Regional Trail. The regional trail alignment as shown in Figure 2 should be acknowledged in the comprehensive plan.
- **Central Anoka Regional Trail:** This is a regional trail that includes segments that are open to the public as well as planned segments that will be developed in the future. The regional trail travels through Ramsey, Anoka, Andover, Ham Lake, Blaine, Lino Lakes, and Centerville as it connects Mississippi West Regional Park, Rum River Regional Trail, Bunker Hills Regional Park, East Anoka County Regional Trail and Rice Creek Chain of Lakes Park Reserve. The regional trail alignment as shown in Figure 2 should be acknowledged in the comprehensive plan.

- **Chain of Lakes-Otter Lake Regional Trail:** This is a regional trail that includes segments that are open to the public as well as planned segments that will be developed in the future. The regional trail travels through Centerville and Lino Lakes as it connects Rice Creek Chain of Lakes Park Reserve and Bald Eagle-Otter Lake Regional Park in White Bear Township. The regional trail alignment as shown in Figure 2 should be acknowledged in the comprehensive plan.
- **Rice Creek North Regional Trail:** This is a regional trail that includes segments that are open to the public as well as planned segments that will be developed in the future. The regional trail travels through New Brighton, Arden Hills, Shoreview, Circle Pines, and Lino Lakes as it connects Long Lake Regional Park, Rice Creek West Regional Trail, Highway 96 Regional Trail, Lexington Parkway Regional Trail Search Corridor, Rice Creek Chain of Lakes Park Reserve, and East Anoka County Regional Trail. The regional trail alignment as shown in Figure 2 should be acknowledged in the comprehensive plan.

Please contact Anoka County for more information regarding Regional Parks and Trails System Components in Lino Lakes.

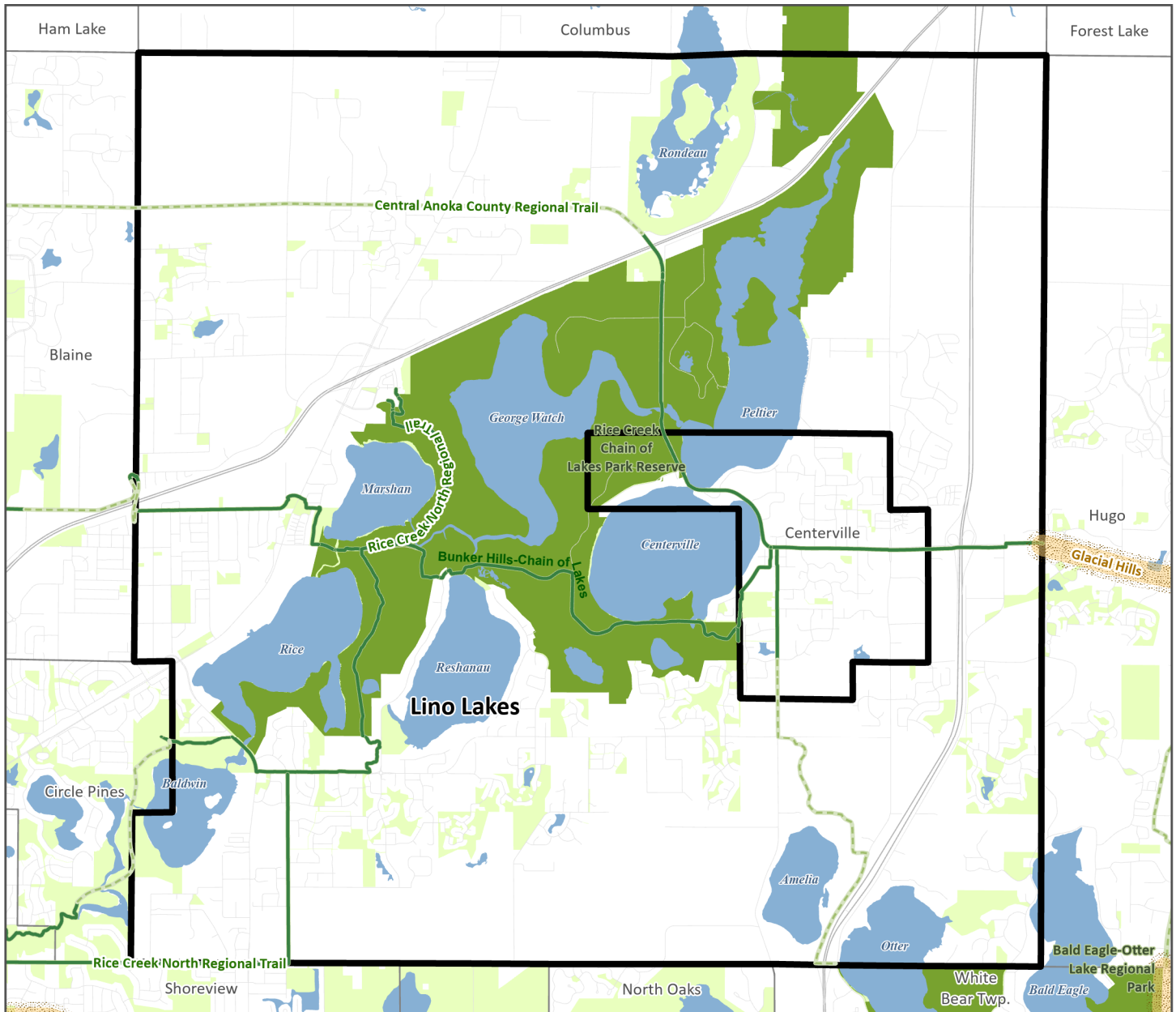
Regional Parks and Trails System



Long-Range Plan Boundary Adjustments and Search Areas

- Boundary Adjustment
- Park Search Area
- Special Feature Search Area
- Regional Trail Search Corridor
- Regional Trail (Open to the Public)
- Planned Regional Trail (Not Open to the Public)
- State Trails
- Regional Park (Open to the Public)
- Planned Regional Park (Not Open to the Public)
- Lower St. Croix National Scenic Riverway
- Mississippi National River & Recreation Area
- State Wildlife Management Areas
- State Parks and Recreation Areas
- MN Valley National Wildlife Refuge
- Special State Recreation Features

City of Lino Lakes Regional Parks and Trails System



Regional Parks and Trails Systems

Other State and National Features

- | | | | |
|---|---|---|---|
|  Boundary Adjustment |  Regional Trail (Open to the Public) |  State Trails |  Other Parks, Preserves, Refuges and Natural Areas |
|  Search Area |  Planned Regional Trail (Not Open to the Public) |  State Parks and Recreation Areas |  Lower St. Croix National Scenic Riverway |
|  Special Feature Search Area |  Regional Park (Open to the Public) |  State Wildlife Management Areas |  Mississippi National River & Recreation Area |
|  Regional Trail Search Corridor |  Planned Regional Park (Not Open to the Public) |  Special State Recreation Features |  MN Valley National Wildlife Refuge |

**PLANNING & ZONING BOARD
AGENDA ITEM 7B**

STAFF ORIGINATOR: Michael Grochala, Community Development Director

MEETING DATE: January 14, 2025

TOPIC: Public Works Design Update

CASE NUMBER: N/A

APPLICANT: City of Lino Lakes

OWNER: N/A

REVIEW SCHEDULE: N/A

BACKGROUND

The City is in the process of completing plans and specifications for a new Public Works building to replace the existing facility at 1189 Main Street. Constructed in 1971, the current buildings are substandard and no longer meet the operational needs of the City. This capital improvement project has been under consideration by the City Council for more than 15 years. A site analysis and needs study were completed in 2017, followed by an alternatives review in 2019. In 2025, the City retained Oertel Architects to design the new facility and RJM Construction to serve as construction manager. The design phase is nearing completion, with construction anticipated to begin in May 2026.

The project site is located within the Public/Semi-Public zoning district, where City facilities are a permitted use. Site and building approvals are subject to administrative review, and staff is currently evaluating the plans for conformance with applicable zoning requirements.

The proposed building is approximately 91,000 square feet and will include office and community meeting space, a fleet garage, and mechanics bays. Staff will provide an overview of the project at the upcoming Planning and Zoning Board meeting.

RECOMMENDATION

Discussion Only.

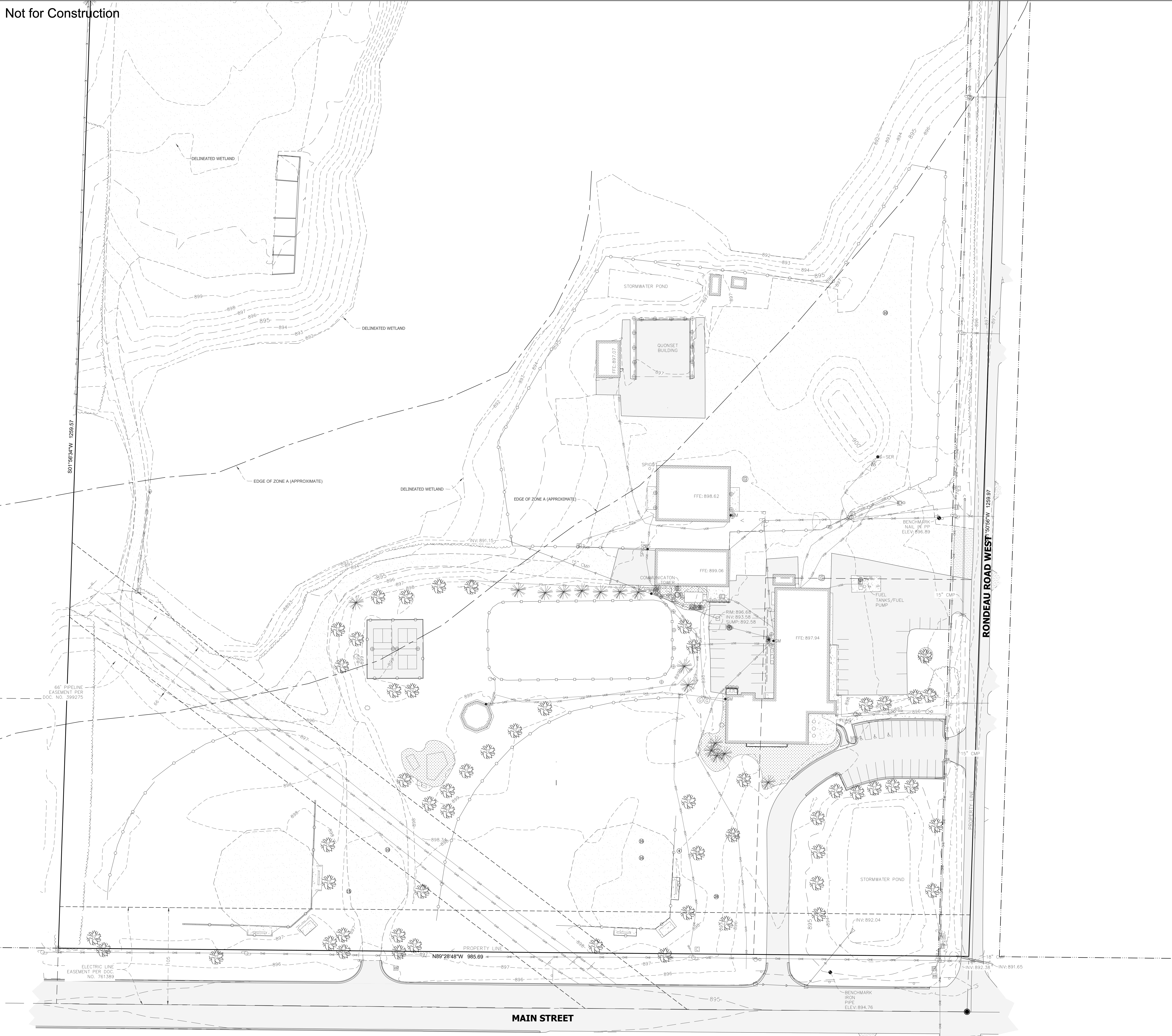
ATTACHMENTS

1. Existing Site Survey
2. Birdseye View

- EXISTING CONDITIONS & TOPOGRAPHIC INFORMATION PROVIDED BY: DESIGN TREE ENGINEERING & LAND SURVEYING 120 17TH AVENUE W ALEXANDRIA, MN 56308
- CONTRACTOR SHALL FIELD VERIFY ALL BUILDING DIMENSIONS AND REMOVAL LIMITS PRIOR TO ANY CONSTRUCTION.
- THE LOCATIONS AND ELEVATIONS OF THE EXISTING UTILITIES SHOWN HEREIN ARE APPROXIMATE. THEY HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND/OR RECORDS. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING LOCATION AND ELEVATION TO ENSURE THAT ANY EXISTING UTILITIES (SHOWN OR NOT SHOWN) ARE NOT DAMAGED DURING CONSTRUCTION.
- GOPHER STATE ONE CALL DAMAGE PREVENTION SYSTEM FOR BURIED UTILITIES. 1-800-252-1166. CONTRACTOR SHALL HIRE A PRIVATE UTILITY LOCATOR TO ASSIST WITH PRIVATE UTILITY LOCATES.

LEGEND

- HYDRANT
- ⊙ SANITARY MANHOLE
- ⊗ GATE VALVE
- ⊕ POWER POLE
- ⊙ LIGHT POLE
- CATCH BASIN
- ⊙ SIGN
- ⊙ DECIDUOUS TREE
- ⊙ CONIFEROUS TREE
- SHRUB
- ⊙ PEDESTAL
- ⊙ GUY WIRE
- ⊙ BOLLARD
- ⊙ POWER BOX
- ⊙ ELECTRIC METER
- ⊙ MONITORING WELL
- ⊙ SANITARY SEWER CLEANOUT
- WOOD FENCE
- CHAINLINK FENCE
- WIRE FENCE
- STORM SEWER LINE
- SANITARY SEWER LINE
- WATERMAIN
- OVERHEAD ELECTRIC
- UNDERGROUND TELEPHONE
- UNDERGROUND FIBER
- UNDERGROUND ELECTRIC
- UNDERGROUND GAS LINE
- ▭ CONCRETE PAVEMENT
- ▭ BITUMINOUS PAVEMENT
- ▭ AGGREGATE SURFACING
- ▭ LANDSCAPING
- ▭ BUILDING



BENCHMARK INFO:
BENCHMARK 1: IRON PIPE
 LOCATED SE CORNER OF SITE. ELEV:894.76
BENCHMARK2: NAIL IN POWER POLE
 LOCATED NE CORNER OF SITE. ELEV:896.89

PROJECT NAME:
LINO LAKES PUBLIC WORKS

1189 MAIN ST., LINO LAKES, MN 55014

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

SIGNATURE: MICHAEL J. GERBER

08/04/2025
 LICENSE # DATE

SHEET NAME:

EXISTING CONDITIONS

REVISIONS:

No.	Date	Description

25-02 PROJECT NUMBER DATE 08/04/2025

N.F. DRAWN BY T.S. CHECKED BY

SHEET NO:

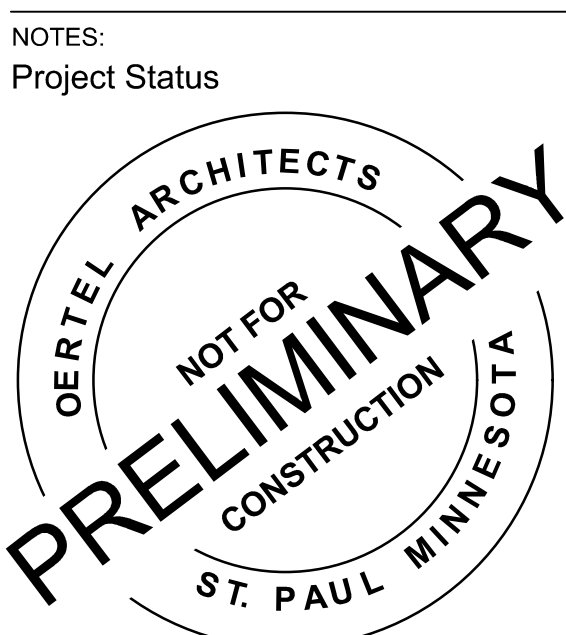
C101

NOTES: Not for Construction

- EXISTING CONDITIONS & TOPOGRAPHIC INFORMATION PROVIDED BY: DESIGN TREE ENGINEERING & LAND SURVEYING
120 17TH AVENUE W
ALEXANDRIA, MN 56308
- CONTRACTOR SHALL FIELD VERIFY ALL BUILDING DIMENSIONS AND REMOVAL LIMITS PRIOR TO ANY CONSTRUCTION.
- SAWCUT CURB AND GUTTER AND SIDEWALK, OR REMOVE AT NEAREST EXPANSION JOINTS.
- SAWCUT BITUMINOUS PAVEMENT FULL DEPTH AT ALL TIE-IN LOCATIONS.
- CONTRACTOR SHALL PLACE ALL NECESSARY EROSION CONTROL MEASURES REQUIRED TO MAINTAIN SITE STABILITY PRIOR TO EXECUTING ANY SITE REMOVALS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH UTILITY PROVIDERS FOR REMOVAL AND/OR RELOCATION OF EXISTING UTILITIES AFFECTED BY SITE DEVELOPMENT. ALL PERMITS, APPLICATIONS, AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL EXCESS OR WASTE MATERIAL GENERATED AS PART OF CONSTRUCTION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS.
- CONTRACTOR SHALL MAINTAIN FULL ACCESS TO ADJACENT PROPERTIES DURING CONSTRUCTION AND TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES.
- THE LOCATIONS AND ELEVATIONS OF THE EXISTING UTILITIES SHOWN HEREIN ARE APPROXIMATE. THEY HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND/OR RECORDS. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING LOCATION AND ELEVATION TO ENSURE THAT ANY EXISTING UTILITIES (SHOWN OR NOT SHOWN) ARE NOT DAMAGED DURING CONSTRUCTION.
- Gopher State ONE CALL DAMAGE PREVENTION SYSTEM FOR BURIED UTILITIES. 1-800-252-1166. CONTRACTOR SHALL HIRE A PRIVATE UTILITY LOCATOR TO ASSIST WITH PRIVATE UTILITY LOCATES.



CONSULTANT:
DESIGN TREE
 engineering + land surveying
 Corporate Office
 120 17th Ave W, Alexandria, MN 56308
 888-216-1916



REMOVALS LEGEND

- = CURB REMOVAL
- = BITUMINOUS PAVEMENT REMOVAL
- = CONCRETE REMOVAL
- = REMOVAL ITEM
- = TREE REMOVAL

KEY NOTES:

- SAWCUT EXISTING BITUMINOUS PAVEMENT
- SAWCUT EXISTING CONCRETE OR REMOVE AT NEAREST EXPANSION JOINT
- SAWCUT EXISTING CURB & GUTTER OR REMOVE AT NEAREST EXPANSION JOINT
- REMOVE EXISTING BITUMINOUS PAVEMENT
- REMOVE EXISTING CONCRETE
- REMOVE EXISTING CURB & GUTTER
- REMOVE TREE
- REMOVE & SALVAGE SIGN
- REMOVE BOLLARD/ POST
- REMOVE STORM STRUCTURE
- REMOVE STORM PIPE
- REMOVE FENCE
- REMOVE GUARD RAIL
- REMOVE CONCRETE MONUMENT
- CANOPY TO BE REPLACED (BY OTHERS)
- RELOCATE UTILITY HAND HOLE (SEE NOTES)



PROJECT NAME:
LINO LAKES PUBLIC WORKS
 1189 MAIN ST., LINO LAKES, MN 55014

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

SIGNATURE: MICHAEL J. GERBER

08/04/2025
 LICENSE # _____ DATE

SHEET NAME:

REMOVALS PLAN

REVISIONS:

No.	Date	Description

25-02 08/04/2025
 PROJECT NUMBER DATE

N.F. T.S.
 DRAWN BY CHECKED BY

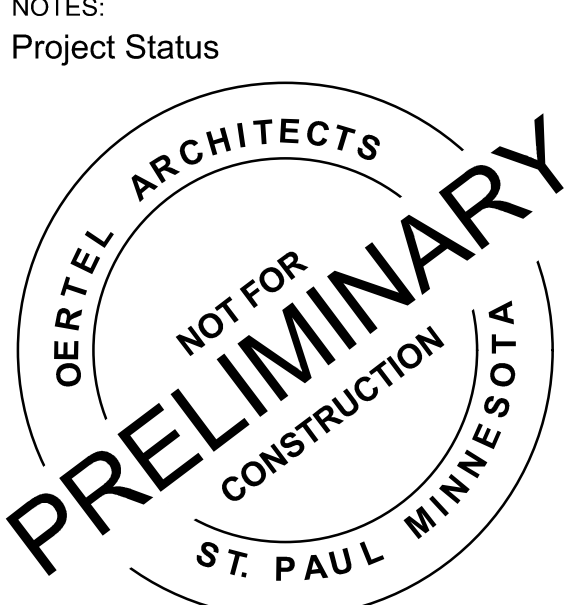
SHEET NO:

- ALL DIMENSIONS SHOWN ARE TO FLOW LINE, CENTERLINE OF FENCE, EDGE OF PAVEMENT, OR EXTERIOR FACE OF BUILDING, UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL VERIFY ALL PLAN AND DETAIL DIMENSIONS PRIOR TO CONSTRUCTION.
- ALL CROSSWALK STRIPING SHALL BE WHITE IN COLOR.
- ALL INTERIOR PARKING STALL STRIPING SHALL BE 4" AND YELLOW IN COLOR.
- ACCESSIBLE PARKING STALL STRIPING, ACCESS AISLE, SYMBOL, AND SIGNAGE SHALL BE IN ACCORDANCE WITH LOCAL AUTHORITY REQUIREMENTS.
- CONTRACTOR SHALL MAINTAIN FULL ACCESS TO ADJACENT PROPERTIES DURING CONSTRUCTION AND TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES.
- ALL SITE WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER.
- CONTRACTOR SHALL PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE PLAN FOR ANY WORK PERFORMED WITHIN THE PUBLIC RIGHT-OF-WAY.
- CONTRACTOR SHALL PROVIDE A TEMPORARY TRAFFIC CONTROL PLAN FOR ANY WORK PERFORMED WITHIN THE PUBLIC RIGHT-OF-WAY.
- GOPHER STATE ONE CALL DAMAGE PREVENTION SYSTEM FOR BURIED UTILITIES. 1-800-252-1166. CONTRACTOR SHALL HIRE A PRIVATE UTILITY LOCATOR TO ASSIST WITH PRIVATE UTILITY LOCATES.

CITY OF LINOLAKES

ARTEL ARCHITECTS, LTD.
1705 Saint Clair Avenue
St. Paul, Minnesota 55108
Phone: 651-946-5186 www.artelarch.com

CONSULTANT:
DESIGN TREE
engineering + land surveying
Corporate Office
120 17th Ave W Alexandria, MN 56308
888-216-1916



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SITE LEGEND

- LIGHT DUTY BITUMINOUS PAVEMENT
- HEAVY DUTY BITUMINOUS PAVEMENT
- CONCRETE PAVEMENT
- CONCRETE SIDEWALK
- CURB AND GUTTER
- TIPPED CURB AND GUTTER
- RETAINING WALL
- CHAINLINK FENCE
- WOOD/VINYL (PRIVACY) FENCE
- TRAFFIC CONTROL SIGNAGE
- PAINTED DIRECTIONAL ARROW
- PAINTED ACCESSIBLE PARKING SYMBOL

PROPERTY INFORMATION

TOTAL PROPERTY AREA	X AC
DISTURBED AREA	X +AC
EXISTING IMPERVIOUS AREA	X AC
PROPOSED IMPERVIOUS AREA	X AC

PARKING INFORMATION

PARKING STALLS	97 STALLS
ACCESSIBLE STALLS	2 STALLS
TOTAL STALLS	99 STALLS

- KEY NOTES:**
- D4 MOUNTABLE CURB AND GUTTER
 - B612 CURB AND GUTTER
 - GUTTER TRANSITION
 - CURB TRANSITION
 - ADA PEDESTRIAN RAMP (SEE DETAILS)
 - TRUNCATED DOMES (SEE DETAILS)
 - CONCRETE DRIVE APRON
 - CROSSWALK STRIPING
 - PARKING STRIPING
 - CHAIN LINK FENCE
 - GATE

PROJECT NAME:
LINO LAKES PUBLIC WORKS

1189 MAIN ST., LINO LAKES, MN 55014

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SIGNATURE: MICHAEL J. GERBER
DATE: 08/04/2025
LICENSE # _____ DATE _____
SHEET NAME:

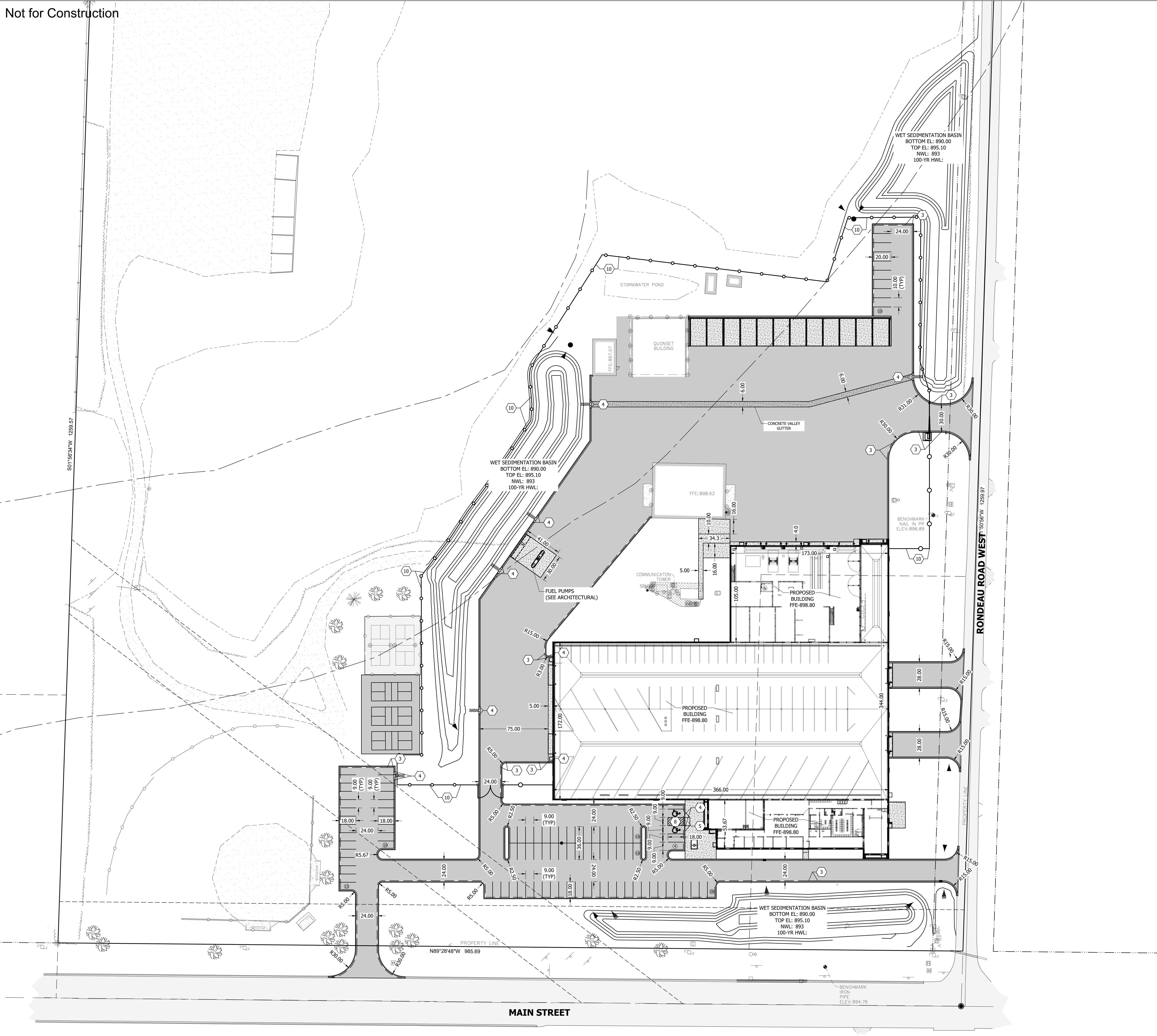
SITE PLAN

REVISIONS:

No.	Date	Description

25-02 PROJECT NUMBER DATE 08/04/2025
N.F. DRAWN BY T.S. CHECKED BY
SHEET NO:

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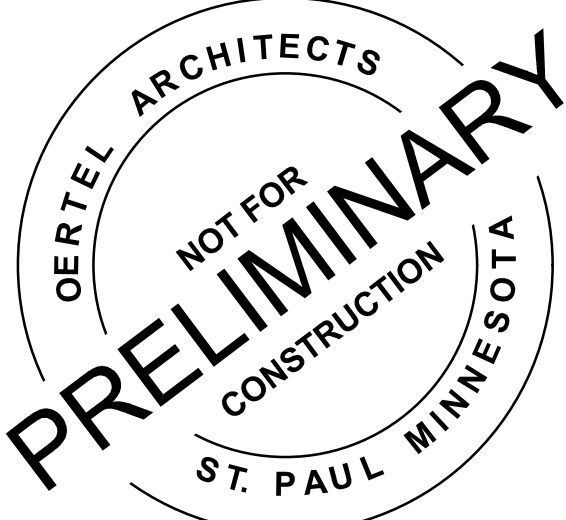


1. THE LOCATIONS AND ELEVATIONS OF THE EXISTING UTILITIES SHOWN HEREIN ARE APPROXIMATE. THEY HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND/OR RECORDS. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING LOCATION AND ELEVATION TO ENSURE THAT ANY EXISTING UTILITIES (SHOWN OR NOT SHOWN) ARE NOT DAMAGED DURING CONSTRUCTION.
2. SIDEWALKS SHALL MEET ADA REQUIREMENTS, AND SHALL NOT EXCEED 2.00% CROSS SLOPE, OR 5.00% LONGITUDINAL SLOPE.
3. CONCRETE ENTRANCES AND APPROACHES SHALL NOT EXCEED 2.00% CROSS SLOPE IN SIDEWALK AREAS.
4. ACCESSIBLE PARKING STALLS SHALL MEET ADA REQUIREMENTS, AND SHALL NOT EXCEED 2.00% SLOPE IN ALL DIRECTIONS.
5. PEDESTRIAN RAMPS SHALL MEET ADA REQUIREMENTS.
6. ALL EXCESS OR WASTE MATERIAL GENERATED AS PART OF CONSTRUCTION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS.
7. ALL EXCAVATION SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF STANDARD SPECIFICATIONS FOR TRENCH EXCAVATION AND BACKFILL/SURFACE RESTORATION* AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
8. IN ADDITION TO THESE PLANS, A STORMWATER MANAGEMENT STUDY HAS BEEN PROVIDED. THE STORMWATER MANAGEMENT STUDY INCLUDES ADDITIONAL INFORMATION REGARDING THE DESIGN OF THE STORMWATER MANAGEMENT BMP(S). THE CONTRACTOR SHALL REVIEW THE STORMWATER BOOK AND COMPLY WITH ALL STATE AND LOCAL REQUIREMENTS.
9. ALL SITE WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER.
10. INFILTRATION AREAS SHALL NOT BE EXCAVATED TO FINAL GRADE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN CONSTRUCTED AND STABILIZED. ONLY LOW IMPACT TRUCK EQUIPMENT SHALL BE USED WITHIN INFILTRATION AREAS.
11. SPOT ELEVATIONS SHOWN INDICATE FINISHED PAVEMENT ELEVATIONS & GUTTER FLOW LINE UNLESS OTHERWISE NOTED. PROPOSED CONTOURS ARE TO FINISHED SURFACE GRADE.
12. COPPER STATE ONE CALL DAMAGE PREVENTION SYSTEM FOR BURIED UTILITIES. 1-800-252-1166. CONTRACTOR SHALL HIRE A PRIVATE UTILITY LOCATOR TO ASSIST WITH PRIVATE UTILITY LOCATES.



CONSULTANT:
DESIGN TREE
 engineering + land surveying
 Corporate Office
 120 17th Ave W, Anandapolis, MN 55308
 888-216-1916

NOTES:
 Project Status



GRADING LEGEND

	= EXISTING MAJOR CONTOUR
	= EXISTING MINOR CONTOUR
	= EXISTING CONTOUR LABEL
	= PROPOSED MAJOR CONTOUR
	= PROPOSED MINOR CONTOUR
	= PROPOSED CONTOUR LABEL
	= EXISTING SPOT ELEVATION*
	= PROPOSED SPOT ELEVATION*
	= PROPOSED SURFACE FLOW DIRECTION

*SPOT ELEVATIONS ALONG CURB & GUTTER AND OTHER REVEALS ARE TO FLOWLINE, UNLESS OTHERWISE NOTED.

- KEY NOTES:**
- 1 MATCH INTO EXISTING BITUMINOUS PAVEMENT
 - 2 MATCH INTO EXISTING CONCRETE
 - 3 MATCH INTO EXISTING CURB & GUTTER
 - 4 MATCH INTO LIP OF EXISTING CURB & GUTTER
 - 5 GRADE BREAK
 - 6 ADA PEDESTRIAN RAMP (SEE DETAILS)
 - 7 DRIVE APRON

BENCHMARK INFO:
BENCHMARK 1:
 LOCATED SE CORNER OF SITE. ELEV:894.76
BENCHMARK2:
 LOCATED NE CORNER OF SITE. ELEV:896.89

PROJECT NAME:
LINO LAKES PUBLIC WORKS
 1189 MAIN ST., LINO LAKES, MN 55014

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SIGNATURE: MICHAEL J. GERBER

08/04/2025
 DATE

SHEET NAME:

GRADING PLAN

REVISIONS:

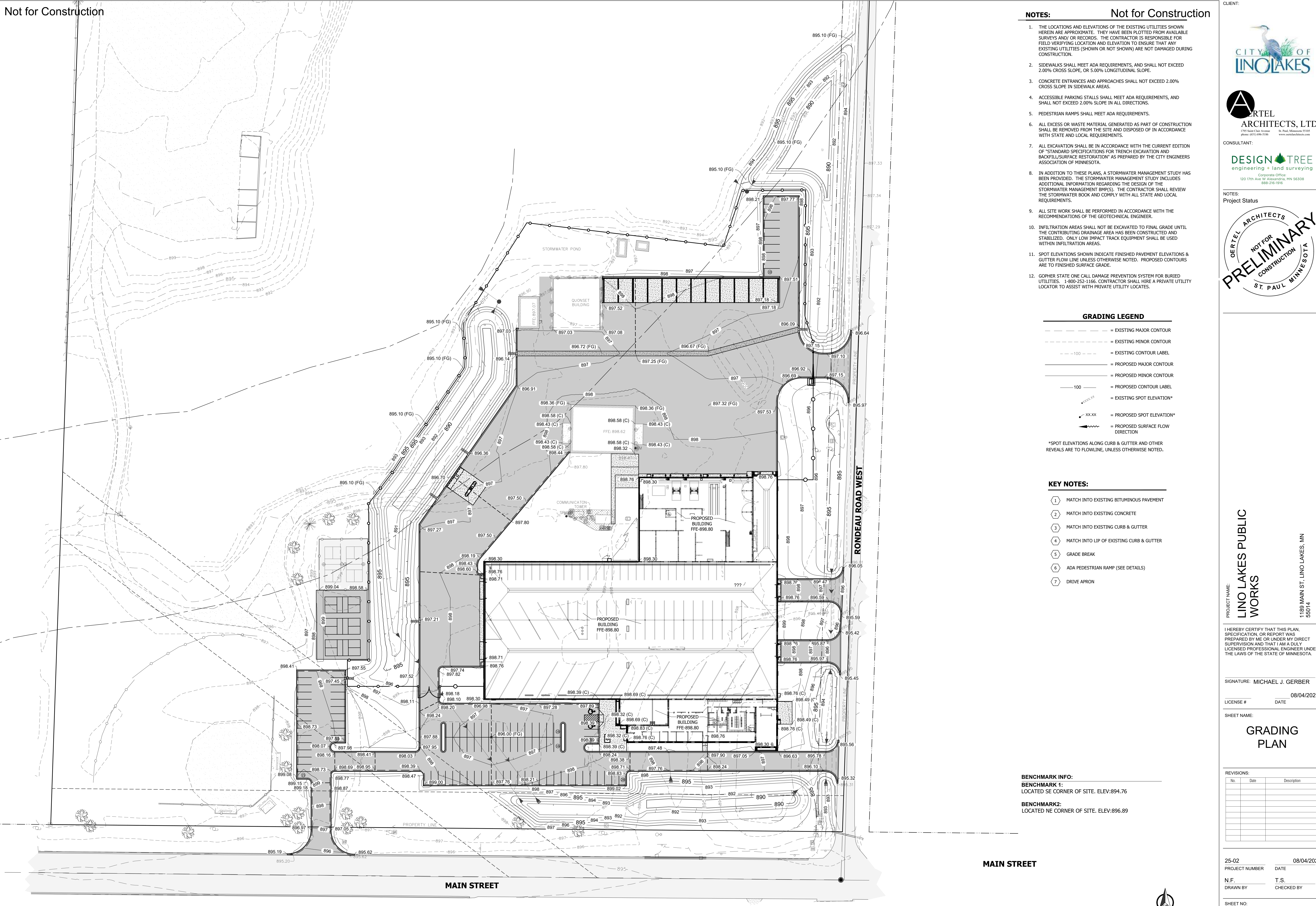
No.	Date	Description

25-02 PROJECT NUMBER DATE 08/04/2025

N.F. DRAWN BY T.S. CHECKED BY

SHEET NO:

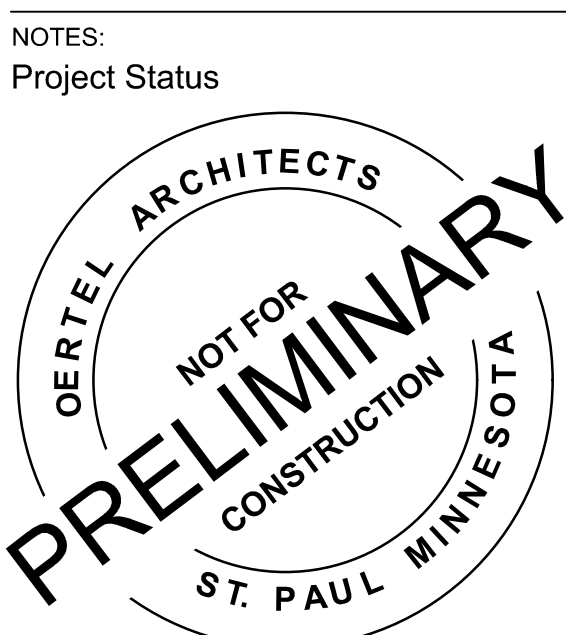
C301



- ALL DISTURBED AREAS SHALL BE FINAL GRADED AND PERMANENTLY STABILIZED WITH THE SEED MIX IDENTIFIED ON PLANS.
- THE SITE MUST BE STABILIZED PER THE REQUIREMENTS OF THE MPCA, NPDES PERMIT, MNDOT, AND THE CITY.
- INLET PROTECTION SHALL BE PROVIDED ON ALL CATCH BASINS AND INLETS DOWN GRADIENT OF CONSTRUCTION ACTIVITY.
- PROVIDE SILT FENCE PERIMETER CONTROL DOWN GRADIENT OF ALL CONSTRUCTION ACTIVITY AND TEMPORARY STOCKPILES.
- TEMPORARY STABILIZED CONSTRUCTION EXITS SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION.
- NO OFFSITE VEHICLE TRACKING IS PERMITTED. STREETS SHALL BE CLEANED AND SWEEPED WHENEVER TRACKING OF SEDIMENTS OCCURS AND BEFORE SITES ARE LEFT IDLE FOR WEEKENDS AND HOLIDAYS.
- REFER TO THE SWPPP AND THE CITY OF _____ EROSION CONTROL REQUIREMENTS FOR FURTHER EROSION CONTROL SEQUENCING.
- IN ADDITION TO THESE PLANS, A STORMWATER MANAGEMENT STUDY HAS BEEN PROVIDED. THE STORMWATER MANAGEMENT STUDY INCLUDES ADDITIONAL INFORMATION REGARDING THE DESIGN OF THE STORMWATER MANAGEMENT BMP(S). THE CONTRACTOR SHALL REVIEW THE STORMWATER BOOK AND COMPLY WITH ALL STATE AND LOCAL REQUIREMENTS.
- WHEN INSTALLING END-OF-LINE FLARED END SECTIONS, BRING THE SILT FENCE UP & OVER THE FLARED END SECTIONS & COVER DISTURBED AREAS WITH RIP RAP. THE UPSTREAM FLARED END SECTIONS SHALL HAVE WOOD FIBER BLANKET INSTALLED ON THE DISTURBED SOILS.
- INFILTRATION AREAS SHALL NOT BE EXCAVATED TO FINAL GRADE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN CONSTRUCTED AND STABILIZED. ONLY LOW IMPACT TRACK EQUIPMENT SHALL BE USED WITHIN INFILTRATION AREAS.
- Gopher State One Call Damage Prevention System for Buried Utilities. 1-800-252-1166. CONTRACTOR SHALL HIRE A PRIVATE UTILITY LOCATOR TO ASSIST WITH PRIVATE UTILITY LOCATES.



CONSULTANT:
DESIGN TREE
 engineering + land surveying
 Corporate Office
 120 17th Ave W, Anandaville, MN 55308
 888-216-1916



PROJECT NAME:
LINO LAKES PUBLIC WORKS
 1189 MAIN ST., LINO LAKES, MN 55014

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

SIGNATURE: MICHAEL J. GERBER

08/04/2025
 LICENSE # DATE

SHEET NAME:
EROSION CONTROL PLAN

REVISIONS:

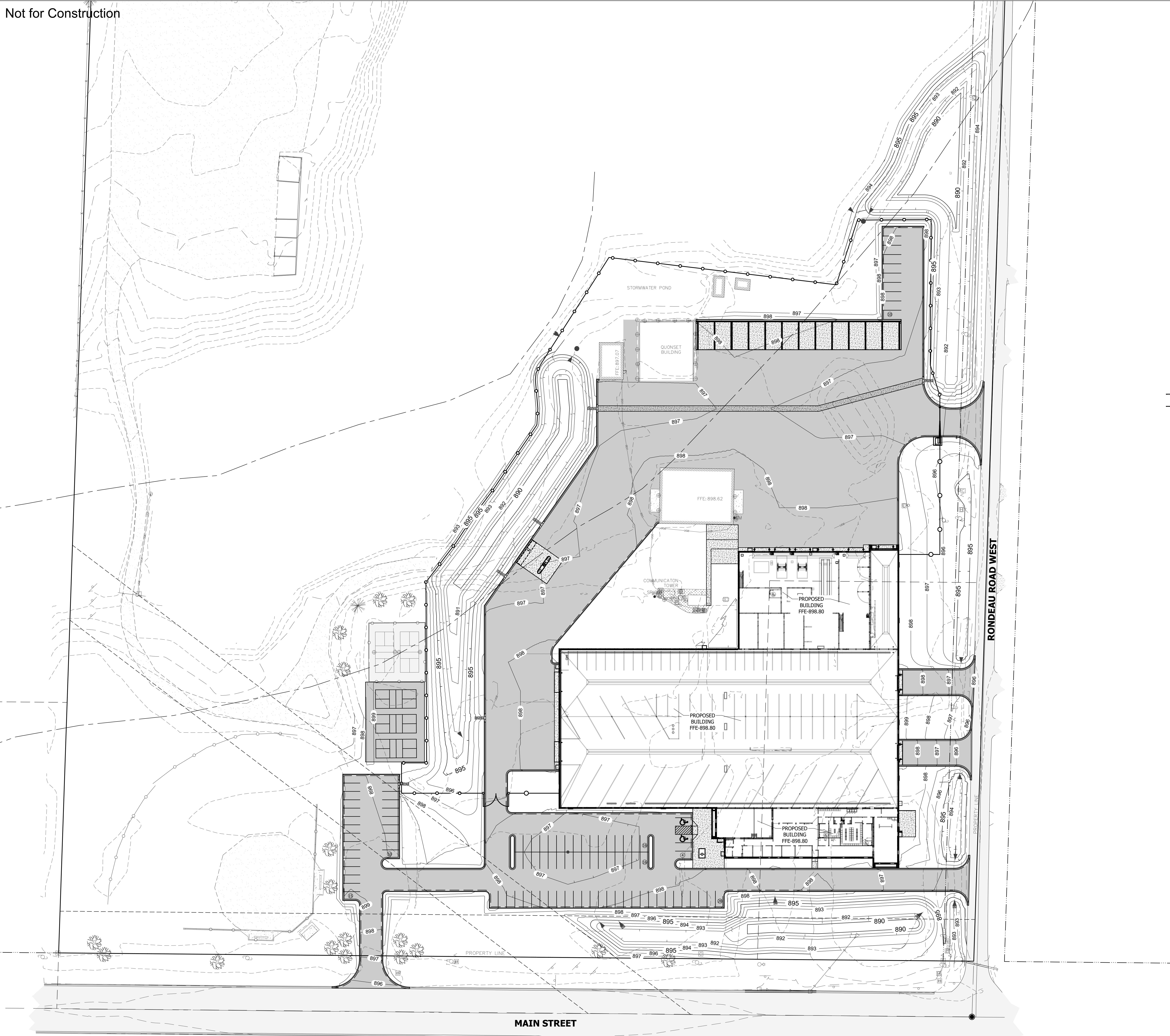
No.	Date	Description

25-02 PROJECT NUMBER DATE 08/04/2025

N.F. DRAWN BY T.S. CHECKED BY

SHEET NO:

C501



EROSION CONTROL LEGEND & QUANTITIES:

	STABILIZED CONSTRUCTION EXIT	(XX EA)
	SILT FENCE	(XX LF)
	SEDIMENT CONTROL ROLL/LOG	(XX LF)
	MNDOT SEED MIX 25-151	(XX AC)
	RIPRAP	(XX SY)
	CATEGORY 20 ROLLED EROSION PREVENTION PRODUCT	(XX SY)
	INLET PROTECTION	(X EA)

NOTE: QUANTITIES SHOWN ARE FOR SWPPP PLAN, AND ARE NOT FOR BIDDING PURPOSES.



